

Daily Report

Project: Date: November 13, 2019 **Durham Meadows Pipeline Installation** Day of Week: Wednesday **Location:** Talcott Ridge Drive, Middletown CT AECOM: 60445033 Report No: 29 Project #: 1 of 11 Page: **Contractor: Ludlow Construction**

Weather Conditions

Conditions: Daylight hours. Sunny, winds variable 5-10 mph.

Temperature (°**F**): High 30°F Low 20°F

Co	ntractor	s & Sub	ocontractors Workforce				Equipment in Use			
Trade	#	x hrs	Trade	#	X	hrs	Туре	#	x	hrs
USACE			Ludlow Construction				Concrete/Asphalt Road Saw	0	X	8
Field Engineer	1	x 8	Superintendent	1	X	8	Dump truck	4	x	8
			Safety Officer	1	X	8	Excavator - Large	1	X	8
			Foreman	1	X	8	Excavator - Mid-Size	1	X	8
			Dump truck Operator	2	X	8	Excavator - Rubber Wheeled - Rock Hammer	0	X	8
			Excavator Operator	2	X	8	Skid Steer / Bucket Loader	1	X	8
			Skid Steer/Bucket Loader Op.	1	X	8	Rolling Compactor	1	X	8
			Laborers	4	X	8	Plate Compactor	1	X	8
			Total 9 persons + 1 sub. Trucker				Jumping Jack Compactor	1	X	8
			Note that some persons operate				Street Sweeper (Broce Broom)	1	X	8
			Multiple pieces equipment.				Material Hopper	1	X	8
			Sub-contractor				Pipe saw	0	X	8
			Dump truck Operator	1	X	8	Light Tower	0	X	8
			Middletown Police Dept.							
			Police Officer Detail	1	X	8				

Visitors: NONE

Notes:

Begin at 31+79.04 and End at 29+95.54 – ADJUSTED Distance. Note: the initial starting point at the junction of Talcott Ridge Dr. and So. Main Street (RTE. 17) was off by 4.83 feet. Initially on October 7, 2019 the starting point was reported as 51+72, however, after a review by Ludlow, that distance should have been 51+76.83. Moving forward the distance will be adjusted, however, Stations reported in Daily Daytime reports from October 7 through 29, 2019 will be off by 4.83 feet as reported. That means 4.83 will have to be added (0+4.83) to all stations during this time frame. Distance counts down as one proceeds up Talcott Ridge Dr. toward the cul-de-sac. Distance is also by length of pipe, not "road distance".

Road Distance vs. Pipe distance: This has been an on-going issue compounded by the direction the station numbers are laid out (i.e. counting down as one proceeds up Talcott Ridge Drive. Just as a point of clarification, the difference in numbers is related to the physical track of the pipe, which is not strait and level as the road distance shown on plan. Both vertical pitch and horizontal deflection/bends contribute to the distance. For reference, "pipe" distance at station 34+04.62 matches to "road" distance station of 34+11. Distances reported in the daytime daily reports are pipe distance.

Pipe Connections: Restrained 20" DI pipe connections for 183.5 feet today from 31+79.04 to 29+95.54. This includes an 11.25° bend. **NOTE:** as a correction to the 11-12-19 daily report, ALL pipe installed on 11-12-19 was restrained.

Compaction Testing: No compaction testing today.

Other: Andrew Beaton of GZA was on site for the day to collect vibration data. Juliano Associates were on site to perform survey activities.

Res. Representative:	J. Meunier	Date:	11/13/19

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0710 – On-site.	Ludlow begins to peel back asphalt from top of trench ar	nd live load it for removal off-site.	
0800 – Meet wi	th Kurt Mintell of the USACE and Jerry Brandon (Ludlo	w contractor) to go over pay items	for 11-12-19.
0848 – Return t	o Talcott Ridge project area. Ludlow has already installe	ed two sticks of 20" pipe via restrai	ned fittings.
0900 – Meet Ar	ndrew Beaton of GZA who is on site for the day to collec	t vibration data. Ludlow installs th	aird stick of 20" pipe.
	installs a 10.5' section of 20" pipe, which is then followed as the perform survey activities along the future pipeling.		
1128 – Ludlow	installs a 9.5' section of 20" pipe, which is connected to	the 11.25° MJ bend with restrained	l joints.
	has installed an additional 2 sticks of 20" pipe after the 9 ipe to mark its location. No other pipe installation activities		
Payment Items	s:		

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Figure 1 – Ludlow removing asphalt from top of trench.

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Figure 2 – Third section of 20" pipe installed.

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Figure 3 – Example of 3"minus gravel with logbook for scale.

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Figure 4 – One of the vibration meters used by GZA today.

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Figure $5 - 11.25^{\circ}$ M.J. bend that was installed between 10.5' and 9.5' sections of 20" pipe.

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Figure 6 – Lowering 9.5' section of 20" pipe to complete 11.25° MJ bend.

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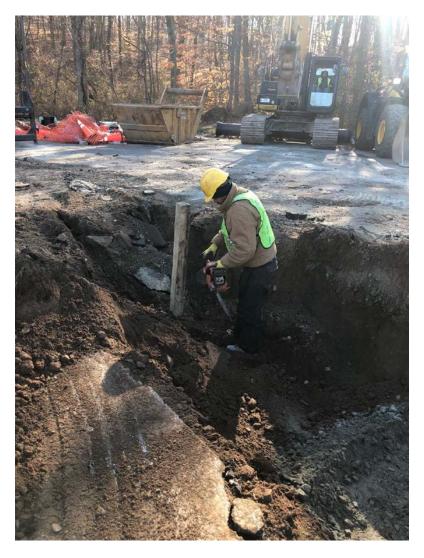


Figure 7 – 4"x4" timber placed at end of pipe run to mark its location. Talcott Ridge Road cul de sac; view to the southeast.

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Figure 8 – Installation of 16"x20" MJ reducer and 20"x20"x6" hydrant tee. View to the south.

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Figure 9 – Compacting 3" minus gravel prior to placement and compaction of processed stone.

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