

## Daily Report

<b>Project:</b>	Durham Meadows Pipeline Installation	<b>Date:</b>	November 13, 2019
<b>Location:</b>	Talcott Ridge Drive, Middletown CT	<b>Day of Week:</b>	Wednesday
<b>Project #:</b>	AECOM: 60445033	<b>Report No:</b>	29
<b>Contractor:</b>	Ludlow Construction	<b>Page:</b>	1 of 11

Weather Conditions			
<b>Conditions:</b>	Daylight hours. Sunny, winds variable 5-10 mph.		
<b>Temperature (°F):</b>	High 30°F	Low	20°F

Contractors & Subcontractors Workforce								Equipment in Use							
Trade		#	x	hrs	Trade		#	x	hrs	Type		#	x	hrs	
USACE					Ludlow Construction					Concrete/Asphalt Road Saw		0	x	8	
Field Engineer		1	x	8	Superintendent		1	x	8	Dump truck		4	x	8	
					Safety Officer		1	x	8	Excavator - Large		1	x	8	
					Foreman		1	x	8	Excavator - Mid-Size		1	x	8	
					Dump truck Operator		2	x	8	Excavator - Rubber Wheeled – Rock Hammer		0	x	8	
					Excavator Operator		2	x	8	Skid Steer / Bucket Loader		1	x	8	
					Skid Steer/Bucket Loader Op.		1	x	8	Rolling Compactor		1	x	8	
					Laborers		4	x	8	Plate Compactor		1	x	8	
					Total 9 persons + 1 sub. Trucker					Jumping Jack Compactor		1	x	8	
					Note that some persons operate					Street Sweeper (Broce Broom)		1	x	8	
					Multiple pieces equipment.					Material Hopper		1	x	8	
					Sub-contractor					Pipe saw		0	x	8	
					Dump truck Operator		1	x	8	Light Tower		0	x	8	
					Middletown Police Dept.										
					Police Officer Detail		1	x	8						

<b>Visitors:</b>	NONE
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<p><b>Notes:</b></p> <p><b>Begin at 31+79.04 and End at 29+95.54 – ADJUSTED Distance. Note: the initial starting point at the junction of Talcott Ridge Dr. and So. Main Street (RTE. 17) was off by 4.83 feet. Initially on October 7, 2019 the starting point was reported as 51+72, however, after a review by Ludlow, that distance should have been 51+76.83. Moving forward the distance will be adjusted, however, Stations reported in Daily Daytime reports from October 7 through 29, 2019 will be off by 4.83 feet as reported. That means 4.83 will have to be added (0+4.83) to all stations during this time frame.</b> Distance counts down as one proceeds up Talcott Ridge Dr. toward the cul-de-sac. Distance is also by length of pipe, not “road distance”.</p> <p><b>Road Distance vs. Pipe distance:</b> This has been an on-going issue compounded by the direction the station numbers are laid out (i.e. counting down as one proceeds up Talcott Ridge Drive. Just as a point of clarification, the difference in numbers is related to the physical track of the pipe, which is not strait and level as the road distance shown on plan. Both vertical pitch and horizontal deflection/bends contribute to the distance. For reference, “pipe” distance at station 34+04.62 matches to “road” distance station of 34+11. Distances reported in the daytime daily reports are pipe distance.</p> <p><b>Pipe Connections:</b> Restrained 20” DI pipe connections for 183.5 feet today from 31+79.04 to 29+95.54. This includes an 11.25° bend. <b>NOTE: as a correction to the 11-12-19 daily report, ALL pipe installed on 11-12-19 was <u>restrained</u>.</b></p> <p><b>Compaction Testing:</b> No compaction testing today.</p> <p><b>Other:</b> Andrew Beaton of GZA was on site for the day to collect vibration data. Juliano Associates were on site to perform survey activities.</p>
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<b>Res. Representative:</b>	J. Meunier	<b>Date:</b>	11/13/19
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<b>Contractor:</b>	Ludlow Construction	<b>Page:</b>	2 of 11

0710 – On-site. Ludlow begins to peel back asphalt from top of trench and live load it for removal off-site.

0800 – Meet with Kurt Mintell of the USACE and Jerry Brandon (Ludlow contractor) to go over pay items for 11-12-19.

0848 – Return to Talcott Ridge project area. Ludlow has already installed two sticks of 20" pipe via restrained fittings.

0900 – Meet Andrew Beaton of GZA who is on site for the day to collect vibration data. Ludlow installs third stick of 20" pipe.

0950 – Ludlow installs a 10.5' section of 20" pipe, which is then followed by a 11.25° MJ bend with restrained joints. Juliano Associates are on site to perform survey activities along the future pipeline driveway leading from the cul de sac to the water tank.

1128 – Ludlow installs a 9.5' section of 20" pipe, which is connected to the 11.25° MJ bend with restrained joints.

1310 – Ludlow has installed an additional 2 sticks of 20" pipe after the 9.5' section of 20" pipe. Ludlow places a 4"x4" timber at the end of the last pipe to mark its location. No other pipe installation activities planned for today. Depart site.

### Payment Items:

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<b>Project #:</b>	AECOM: 60445033	<b>Report No:</b>	29
<b>Contractor:</b>	Ludlow Construction	<b>Page:</b>	3 of 11



Figure 1 – Ludlow removing asphalt from top of trench.

<b>Res. Representative:</b>	J. Meunier	<b>Date:</b>	11/13/19
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## Daily Report

<b>Project:</b>	Durham Meadows Pipeline Installation	<b>Date:</b>	November 13, 2019
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<b>Project #:</b>	AECOM: 60445033	<b>Report No:</b>	29
<b>Contractor:</b>	Ludlow Construction	<b>Page:</b>	4 of 11



**Figure 2 – Third section of 20” pipe installed.**

<b>Res. Representative:</b>	J. Meunier	<b>Date:</b>	11/13/19
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<b>Project:</b>	Durham Meadows Pipeline Installation	<b>Date:</b>	November 13, 2019
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<b>Contractor:</b>	Ludlow Construction	<b>Page:</b>	5 of 11



**Figure 3 – Example of 3”minus gravel with logbook for scale.**

<b>Res. Representative:</b>	J. Meunier	<b>Date:</b>	11/13/19
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<b>Project #:</b>	AECOM: 60445033	<b>Report No:</b>	29
<b>Contractor:</b>	Ludlow Construction	<b>Page:</b>	6 of 11



Figure 4 – One of the vibration meters used by GZA today.

<b>Res. Representative:</b>	J. Meunier	<b>Date:</b>	11/13/19
-----------------------------	------------	--------------	----------



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<b>Contractor:</b>	Ludlow Construction	<b>Page:</b>	7 of 11



Figure 5 – 11.25° M.J. bend that was installed between 10.5' and 9.5' sections of 20" pipe.

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<b>Project:</b>	Durham Meadows Pipeline Installation	<b>Date:</b>	November 13, 2019
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<b>Project #:</b>	AECOM: 60445033	<b>Report No:</b>	29
<b>Contractor:</b>	Ludlow Construction	<b>Page:</b>	8 of 11



Figure 6 – Lowering 9.5' section of 20" pipe to complete 11.25° MJ bend.

<b>Res. Representative:</b>	J. Meunier	<b>Date:</b>	11/13/19
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**Date:** November 13, 2019  
**Day of Week:** Wednesday  
**Report No:** 29  
**Page:** 9 of 11



Figure 7 – 4"x4" timber placed at end of pipe run to mark its location. Talcott Ridge Road cul de sac; view to the southeast.

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**Day of Week:** Wednesday  
**Report No:** 29  
**Page:** 10 of 11



Figure 8 – Installation of 16"x20" MJ reducer and 20"x20"x6" hydrant tee. View to the south.



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<b>Contractor:</b>	Ludlow Construction	<b>Page:</b>	11 of 11



Figure 9 – Compacting 3” minus gravel prior to placement and compaction of processed stone.