

Daily Report

Project:	Durham Meadows Pipeline Installation	Date:	November 06, 2019
Location:	Talcott Ridge Drive, Middletown CT	Day of Week:	Wednesday
Project #:	AECOM: 60445033	Report No:	25
Contractor:	Ludlow Construction	Page:	1 of 14

Weather Conditions

Conditions: Daylight hours. Sunny, 2% cloud cover, winds 10-15 mph.
Temperature (°F): High 54°F Low 39°F

Contractors & Subcontractors Workforce				Equipment in Use			
Trade	#	x	hrs	Trade	#	x	hrs
USACE				Ludlow Construction			
Field Engineer	1	x	8	Superintendent	1	x	8
				Safety Officer	1	x	8
				Foreman	1	x	8
				Dump truck Operator	2	x	8
				Excavator Operator	2	x	8
				Skid Steer/Bucket Loader Op.	1	x	8
				Laborers	4	x	8
				<i>Total 9 persons + 1 sub. Trucker</i>			
				<i>Note that some persons operate</i>			
				<i>Multiple pieces equipment.</i>			
				Sub-contractor			
				Dump truck Operator	1	x	8
				Middletown Police Dept.			
				Police Officer Detail	1	x	8

Visitors: Steve Dunbar (USACE)

Notes:

Begin at 35+63.95 and End at 34+04.62 (Note: 38+28.78 [adjusted] to 35+63.95 was installed on Monday and Tuesday 11/4-5) – ADJUSTED Distance. Note: the initial starting point at the junction of Talcott Ridge Dr. and So. Main Street (RTE. 17) was off by 4.83 feet. Initially on October 7, 2019 the starting point was reported as 51+72, however, after a review by Ludlow, that distance should have been 51+76.83. Moving forward the distance will be adjusted, however, Stations reported in Daily Daytime reports from October 7 through 29, 2019 will be off by 4.83 feet as reported. That means 4.83 will have to be added (0+4.83) to all stations during this time frame. As always, distance counts down as one proceeds up Talcott Ridge Dr. toward the cul-de-sac. Distance is also by length of pipe, not “road distance”.

Pipe Connections: Restrained pipe connections for all pipes were installed today. A total of 159.33 feet of 16” DI pipe was placed. The odd number comes from having to cut off the restraining bead on the first pipe because it was restrained using a Mega-lug to a 16” butterfly valve. The butterfly valve was the ending station point yesterday, 11/5/19. Note that 364.77 feet were installed on Monday and Tuesday of this week (11/4-5). Thursday and Friday of last week (10/31-11/1) was dedicated to installing 217 feet of 8-inch DIP up “lower” Watch Hill Drive and the installation of the first of 2 hydrants to be replaced on Watch Hill. A copy of Billy’s (Ludlow) notes are include in the figures for 10/31 and 11/1. Still awaiting completed notes for 11/4-5.

Compaction Testing: Compaction testing was conducted yesterday (11/5) on the 3 lowest lifts and today (11/6) on the two upper lifts. Testing was not completed yesterday due to timing of materials and of the testing company. All lifts tested passed on first test.
11/5/19 First Lift 3” Processed 97.3% compaction with 2.2% moisture at Station 36+54
11/5/19 Second Lift 3” Processed 96.1% compaction with 3.9% moisture at Station 36+65
11/5/19 Third Lift 3” Processed 100.1% compaction with 3.4% moisture at Station 36+87
11/6/19 Fourth Lift 1” Processed 100.5% compaction with 2.1% moisture at Station 35+34
11/6/19 Fifth Lift 1” Processed 100.5% compaction with 1.9% moisture at Station 35+52

Res. Representative:

W. Abrahams-Dematte

Date: 11/06/19

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Correction: Daytime Daily Reports for 10/29 and 10/30. Although no 16 DI Pipe was installed on these days, the initial and ending stations were reported (as the same) for both days. However, a typo in carrying forward from 10/28 inadvertently added 100 feet back to the numbers. To add confusion a length correction was made by Ludlow on 10/29. The reported values for 10/29-30 were 39+23.95 unadjusted and 39+28.78 adjusted. The corrected values should be 38+23.95 unadjusted and 38+28.78 adjusted.

1130 – On-site. Discussion with Billy (Ludlow) confirmed the days starting point was at the 16” butterfly valve that was the uphill end of the 8” Tee-assembly for the “upper” Watch Hill Drive hydrant replacement line. This was station 35+63.95. Billy noted that on Thursday and Friday of last week (10/31-11/1) they installed 217 feet of 8” DI up “lower” Watch Hill Drive and installed a hydrant. He also went on to note that they installed an extra bend to get up the hill. Monday and Tuesday of this week (11/4-5) they installed 16” DI up Talcott Drive ending on Tuesday 11/5 with a 16x8-inch tee with 8-inch gate valve and two 16” butterfly valves on either end (same as the assembly for “lower” Watch Hill Dr.). Based on today's starting station and Wednesday's (10/30) ending station (38+28.78 adjusted) – no 16” DIP was installed on Thursday and Friday of last week – 264.83 feet was installed on Talcott Ridge on Monday and Tuesday of this week. Notes from Billy (Ludlow) are included in the figures for 10/31 and 11/1. Notes for 11/4-5 are pending and should be available later this week.

1145 – Talking with Andy (Ludlow), 100 feet of 16” DI has been installed thus far. Currently prepping for next 20 feet.

1200 – Installing 6th stick of 16” x 20 feet of DI pipe. Compaction testing company on site. Conducting tests on upper two lifts. Lower lifts were tested on 11/5.

1220 – Installing 7th stick of 16” x 20 feet DI pipe. Stone dust appears a bit browner today (more sand content). Pipe depths are becoming shallower now 5.5 feet to top of pipe below grade.

1300 – Burying 8th and final stick of the day. Total install of 16” DI is 159.33 feet of pipe. First section of day was shortened - bead cut off to allow use of Mega-lug connection to 16” butterfly valve. Ending station 34+04.62. Beginning the wind down of filling remaining trench and hot patching, and street sweeping.

1330 –Off site.

Payment Items:

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Figure 1 – Facing south, today's starting station 35+63.95 on Talcott Ridge. To the right of the photo is 'upper' Watch Hill Drive. Cap seen at start of road cut is the valve access to a 16" butterfly valve, one of two in the 16x8" tee assembly, which was the final install yesterday 11/5. The tee will connect an 8" line up Watch Hill to a hydrant replacement.

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Figure 2 – Facing west – Today's starting point, note valve access cap in center of shot. The other two valve access caps can be seen faintly in the asphalt to the right, creating a triangle that points up 'upper' Watch Hill in the background of the shot.

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Figure 3 – Facing southwest. Running into ledge along the trench – indicated by the ‘smoke’. Slows progress due to the need to dig it out.

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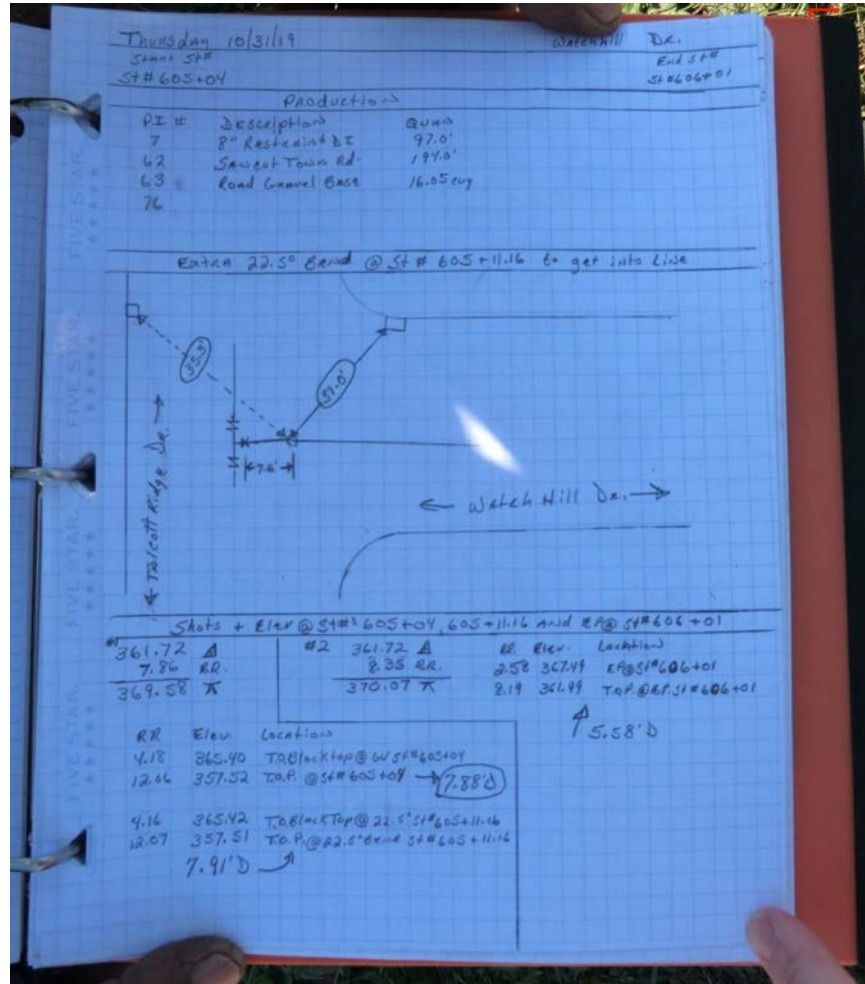


Figure 4 – Thursday 10/30/19 Details. Source is Billy L. (Ludlow) notes.

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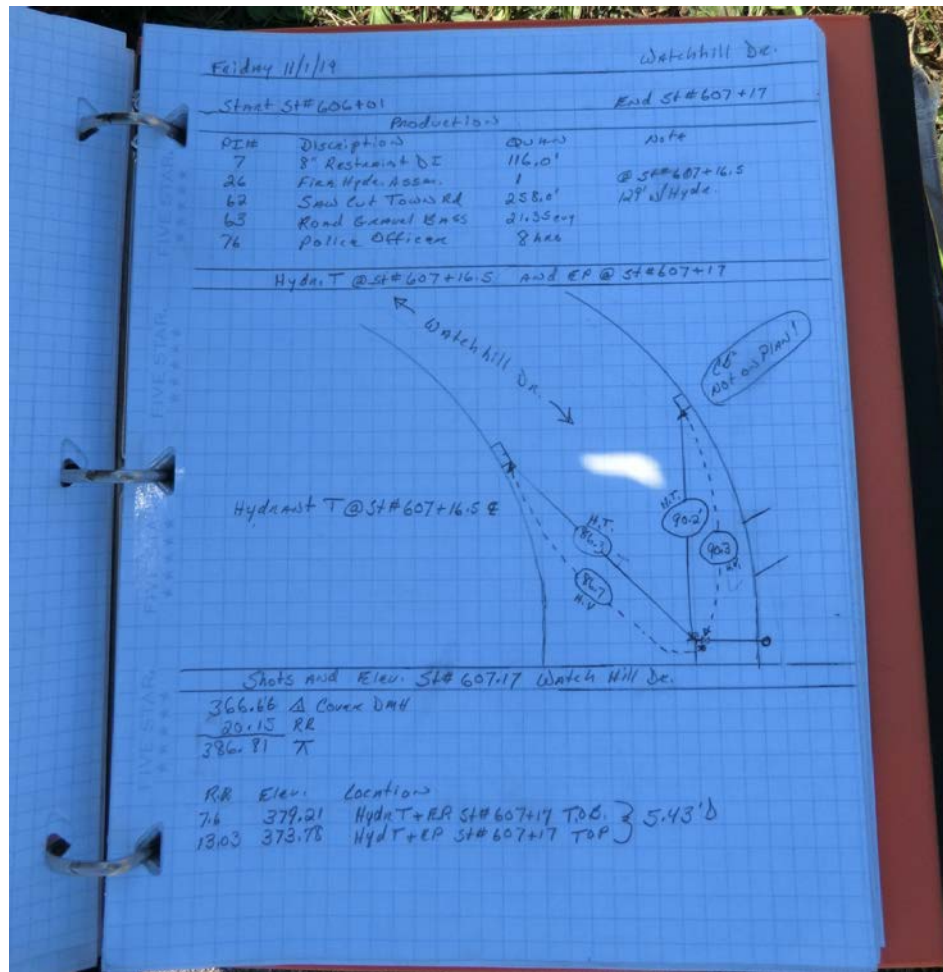


Figure 5 – Friday 11/1/19 Details. Source is Billy L. (Ludlow) notes.

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Figure 6 – Compaction testing. Upper two lifts done today, bottom three lifts done yesterday.

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Figure 7 – Facing west. Running the Wacker compaction plate. Upper right of the photo is ‘upper’ Watch Hill Drive.

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Figure 8 – Typical. Preparing for next stick of 16” DIP to be installed.

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Figure 9 – Pipe restraint lock (orange). Also noticeable in this picture is the cylindrical rubber spacer (behind the orange lock) that keeps the orange and partnering black lock (not shown) in place while pipe is adjusted and buried. There are two sets, one shown here, the other 180° around the pipe on the other side.

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Figure 10 – Facing south, last run of pipe for the day ending at Sta. 34+04.62. Running Water Line Caution tape over top of 16" DIP

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Figure 11 – Facing south, pile of 1” process waiting to be installed in the final lifts of trench.

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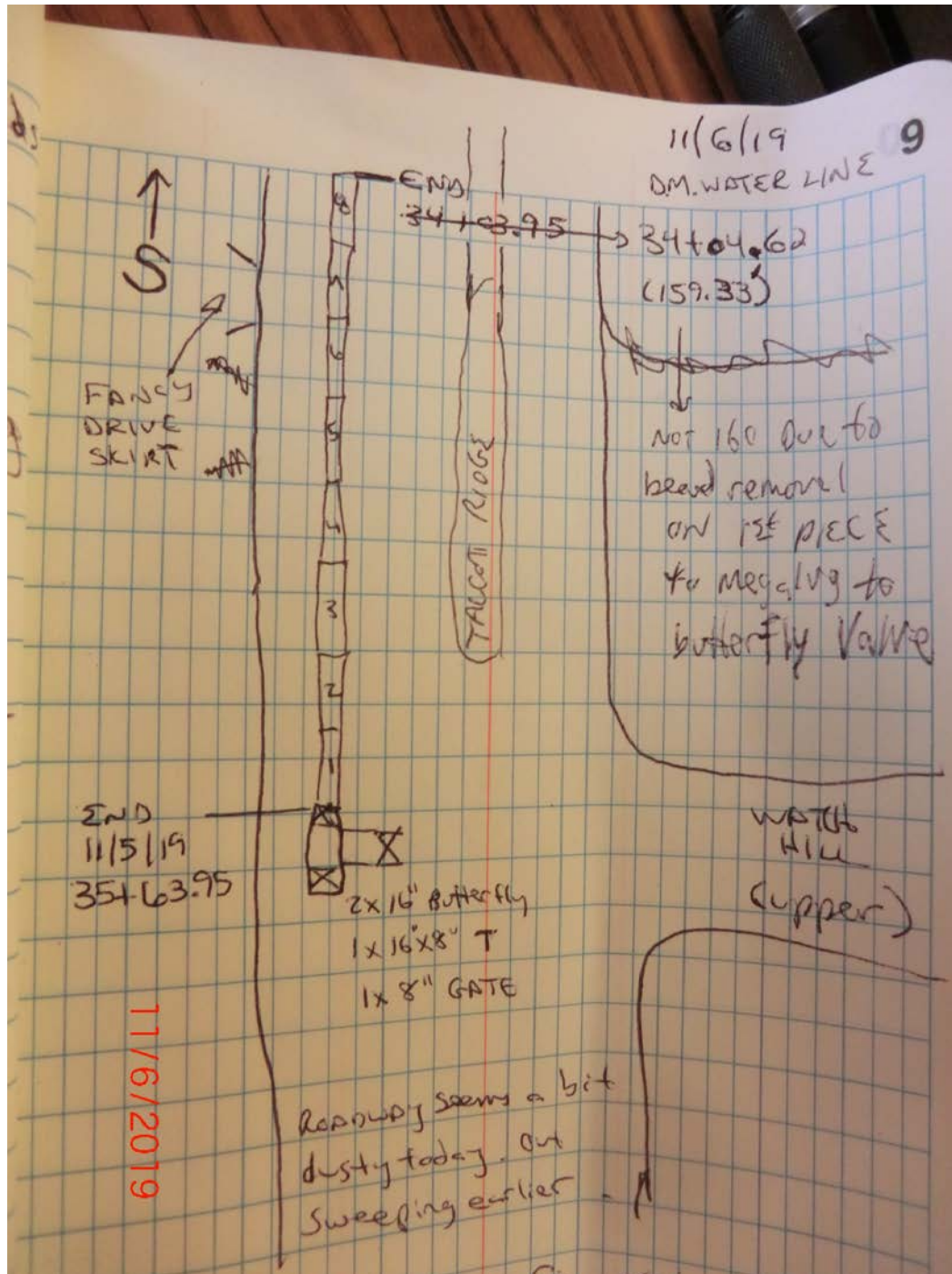


Figure 12 – Today's pictogram of work. Note Upper Watch Hill refers to the intersection closest the Talcott cul-de-sac. Lower would refer to the first intersection with Watch Hill closest to Main Street.