

Daily Report

Project:	Durham Meadows Pipeline Installation	Date:	October 29, 2019
Location:	Talcott Ridge Drive, Middletown CT	Day of Week:	Tuesday
Project #:	AECOM: 60445033	Report No:	23
Contractor:	Ludlow Construction	Page:	1 of 18

Weather Conditions

Conditions: Daylight hours. 100% overcast and drizzly all day.
Temperature (°F): High 57°F Low 47°F

Contractors & Subcontractors Workforce				Equipment in Use			
Trade	#	x	hrs	Trade	#	x	hrs
USACE				Ludlow Construction			
Field Engineer	1	x	8	Superintendent	1	x	8
				Safety Officer	1	x	8
				Foreman	1	x	8
				Dump truck Operator	2	x	8
				Excavator Operator	2	x	8
				Skid Steer/Bucket Loader Op.	1	x	8
				Laborers	4	x	8
				<i>Total 9 persons + 0 sub. Trucker</i>			
				<i>Note that some persons operate</i>			
				<i>Multiple pieces equipment.</i>			
				Sub-contractor			
				Dump truck Operator	0	x	8
				Middletown Police Dept.			
				Police Officer Detail	1	x	8

Visitors: NONE

Notes:

Begin at 39+23.95 and End at 39+28.78 – No 16” DI installed – ADJUSTED Distance. Note: the initial starting point at the junction of Talcott Ridge Dr. and So. Main Street (RTE. 17) was off by 4.83 feet. Initially on October 7, 2019 the starting point was reported as 51+72, however, after a review by Ludlow, that distance should have been 51+76.83. Moving forward the distance will be adjusted, however, Stations reported in Daily Daytime reports from October 7 through 29, 2019 will be off by 4.83 feet as reported. That means 4.83 will have to be added (0+4.83) to all stations during this time frame. As always, distance counts down as one proceeds up Talcott Ridge Dr. toward the cul-de-sac. Distance is also by length of pipe, not “road distance”.

Pipe Connections: No 16” DIP was installed today due to lack of available pipe. Ludlow is awaiting delivery, but in the meantime installed 2 fire hydrants along Talcott Ridge.

Compaction Testing: No Compaction testing today.

Sidewalk Closed: Though not captured in any of the pictures today or yesterday, closed sidewalk signs were in used to deter pedestrians from crossing through the construction area.

Haul Road: Drive throughs along the haul road and stockpile area shows that they appear to be in good shape (no large piles of gravel along route). Catch basin at corner of driveway at the material staging area has been fitted with a silt prevention bag.

Truck Hauling Note: One of the drivers was pulled over at Route 147 Middlefield Rd./Durham Rd/ Main St. and Route 157 Reeds Gap Rd. by DOT for weight limitation violation. The DOT weighed the vehicle and fined the driver for being overweight.

Res. Representative:

W. Abrahams-Dematte

Date: 10/29/19

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0830 – On site: Due to delay in pipe shipments, Ludlow is installing fire hydrants today. First location is 40+44.38 (Adjusted number 40+49.21). Hole for hydrant already dug and being prepped for installation.

0900 – Installing 6” hydrant pipe to Tee. Restrained connections.

0910 – Installing hydrant assembly. Note that the assembly came with a non-restrained gland. Ludlow swapped out the manufacturer gland for a Mega-Lug fitting with the check lugs.

1030 – Finished backfilling and compacting first hydrant location. Moved to second location and took break early.

1230 – Installing second fire hydrant at 41+42.34 (Adjusted location 41+47.17). Again, replaced manufacturer gland with Mega-Lug restrained gland.

1300 – Beginning lifts of processed fill on hydrant number 2. Also marking out the hydrant spur line up Watch Hill Drive from the first intersection with Watch Hill. The mark out is for saw cutting tomorrow in preparation to install 8” line up to the hydrant-Tee for the new hydrant on Watch Hill.

1330 – Discussion with Billy (Ludlow) – the initial starting point at the intersection of South Main St. (Rte. 17) and Talcott Ridge Drive was off by 4.83 feet. Instead of starting at 51+72 the actual start was 51+76.83; which will carry all the way to the top. See notes at top of page for Daily Daytime Report dates affected.

1400 – Departed MMC trailers for one more haul road drive-by. Off site.

Payment Items:

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Figure 1 – Hydrant Assemblies. First location on Talcott Ridge. Station Old 40+44.38 / New 40+49.21

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Figure 2 – The man in the box. Preparing base for hydrant line. 6" gate valve (red) seen in center of picture.

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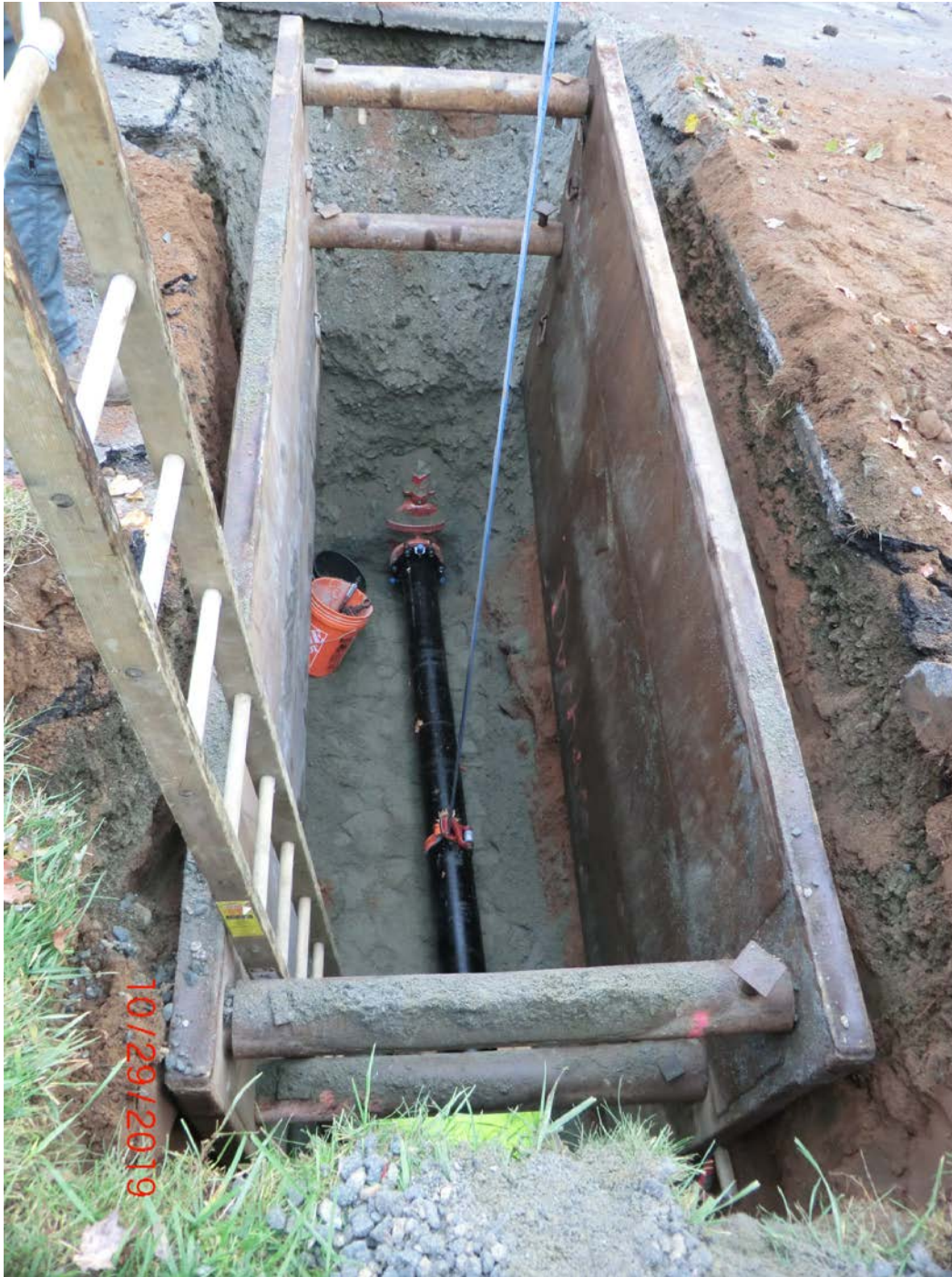


Figure 3 – Setting 6” line with Mega-Lug restraint to gate valve.

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Figure 4 – Awaiting Hydrant Assembly

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Figure 5 – Removing manufacturer gland. Note bolts already removed, laying on grass.

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Figure 6 – Hydrant connection with manufacturer gland removed – on grass with bolts. In upper right of picture is the Mega-Lug restraint with check lugs (blue).

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Figure 7 – Completed hydrant assembly connection. Note one 8” thrust block set behind elbow and one 8” block set under the elbow for support (mostly hidden by stone dust, only edge visible).

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Figure 8 – Street valve connector. Top of gate valve.

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Figure 9 – Valve box placed over street valve connector (pictured in previous figure).

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Figure 10 – Crushed stone backfill around base of hydrant and stone dust over pipe. Crushed stone covered by geotextile fabric and entire length covered in stone dust. Compacted and then covered with processed fill compacted in lifts.

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Figure 11 – Mis-communication. Processed fill dropped up the street near yesterdays ending point. Driver unaware that water main pipe was not being installed today. Fill was bucket loaded and delivered to the hydrant installations. Created a wider spread area to clean at the end of the day.

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Figure 12 – Backfilling with processed backfill. On right side of picture note bag over hydrant to indicate not in use. Left side of picture is valve extension and riser pipe; note cap on upside down to prevent processed fill from getting into riser.

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Figure 13 – Existing fire hydrant and new fire hydrant to be installed. Second location at Station Old 41+42.34 / New 41+47.17.

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Figure 14 – Awaiting hydrant assembly. Note 8” block for elbow to set on (bottom of picture).

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Figure 15 – Attaching hydrant assembly to 6” line with Mega-Lug restraint.

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Figure 16 – First hydrant location, awaiting asphalt patch.