

## Daily Report

<b>Project:</b>	Durham Meadows Pipeline Installation
<b>Location:</b>	Talcott Ridge Drive, Middletown CT
<b>Project #:</b>	AECOM: 60445033
<b>Contractor:</b>	Ludlow Construction

**Date:** October 08, 2019  
**Day of Week:** Monday  
**Report No:** 10  
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**Weather Conditions 100% Overcast All Day. Drizzle stopped by 0900**

<b>Conditions:</b>	Day			
<b>Temperature (°F):</b>	High	65°F	Low	51°F

Contractors & Subcontractors Workforce							
Trade	#	x	hrs		Trade	#	x hrs
USACE					Ludlow Construction		
	Field Engineer	1	x	8	Superintendent	1	x 8
					Safety Officer	1	x 8
					Foreman	1	x 8
					Dump truck Operator	1	x 8
					Excavator Operator	2	x 8
					Skid Steer Operator	1	x 8
					Laborers	3	x 8
				Sub-contractor			
				Dump truck Operator	1	x 8	
				Middletown Police Dept.			
				Traffic officer	1	x 8	

Equipment in Use			
Type	#	x	hrs
Concrete sawing machine	0	x	8
Dump truck	4	x	8
Excavator - Large	0	x	8
Skid Steer / Bucket Loader	1	x	8
Plate Compactor	1	x	8
Jumping Jack Compactor	1	x	8
Light Tower	0	x	8
Mid-size Excavator	2	x	8
Tractor-style Excavator	0	x	8
Pipe saw	1	x	8
Rolling Compactor	0	x	8
Street Sweeper (Broce Broom)	1	x	8
Material Hopper	1	x	8

## Summary of Work

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**Visitors:** None

**Notes:**

**Begin at 51+72 and End at 50+91** (distance counts down as one proceeds up Talcott Ridge).

Note that there is a correction to 10/7/19: Begin 51+92 (not +93) and end 51+72. Ludlow foreman made adjustment.

0730 – on site Talcott Ridge Dr. Excavating under way Starting at 51+72

0750 – Most of first 18 feet dug. Located existing fire hydrant line at 4-5 feet below grade 8-inch line. Existing hydrant line located at 51+49.

0830 – Fire hydrant Tee in cut into 18-foot section of 16” DI and Tee assembly attached to one cut end of pipe along with mega-lug restrainer. The second half of cut pipe to be installed in the ground to prevent bending at junction while lowering pipe into pit. First 18 foot run base prep (gravel and bedding) nearly completed.

0840 – Un-corked pipe from previous days installation, released about 20 gallons of water that apparently seeped in overnight. Sump used to evacuate whatever water that did not drain into the gravel. First 18-foot section of pipe installed to 51+52.

0900 – Bedding to one foot above top of pipe. Jumping Jack compacted. Adding gravel backfill soil.

**Res. Representative:** W. Abrahams-Dematte

**Date:** 10/08/19

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0945 – Second 18-foot section of 16" DI installation, includes New Fire Hydrant Tee at 51+38. Top of Hydrant Tee at elevation 349.70', existing blacktop at 355.78' (6.08' below existing grade). Tie-offs taken to nearest corner of double catch basins located at the intersection of Talcott Ridge Rd. and South Main St. (Rte. 17). Distance to catch basin on south side of Talcott Ridge 46.6', and to catch basin on north side of Talcott Ridge Rd. 37.9'.

1035 – Installation of remaining section of second 18-foot run that included the new fire hydrant Tee. End at 51+31

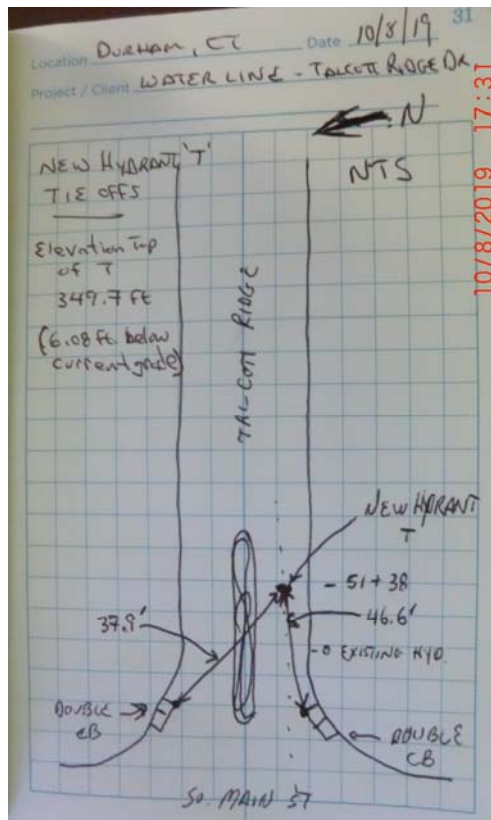
1045-1115 – Coffee Break.

1210 – Third 18-foot section of 16" DI pipe installed to 51+11. Pie bedding dust to 12" above being compacted. Open trench from first 36 feet being filled and compacted simultaneously.

1230 – Road sweeping on traveled side of road to clean up gravel.

1315 – Final 18-foot section of 16" DI pipe installed to 50+91.

1330 – Drove up to top of Talcott Ridge Dr. A lot of pipe and equipment being staged in the cul-de-sac. Gravel is also being stockpiled along Talcott Ridge Rd. near the intersection of Watch Hill Drive. Off-site.



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### Payment Items:



Figure 1 – Start of first run at 51+72. Note excavator to left digging and excavator to right backfilling (not currently active). Also note double catch basin to front left of picture – typical liner installed to catch sediment runoff. Picture taken facing south.



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**Figure 2 – Start of operations – Plugged pipe end from 10/7/19 at 51+72. When this plug was removed approximately 20 gallons water spilled out. Gate valve at other end of pipe (Tee-section with So. Main Street line) is closed at time of this picture. Picture taken facing west into road cut.**

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Figure 3 – Existing fire hydrant line, 6-inch pipe located at approximately 51+49.



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Figure 4 – Fire Hydrant Tee assembly preparation. Pipe cut at approximately 14 feet to insert Tee. Remaining 6 feet at bottom right of picture.



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Figure 5 – Sump discharge to double catch basin (south side of Talcott Ridge Dr. at South Main St.). Note that catch basins have been lined.

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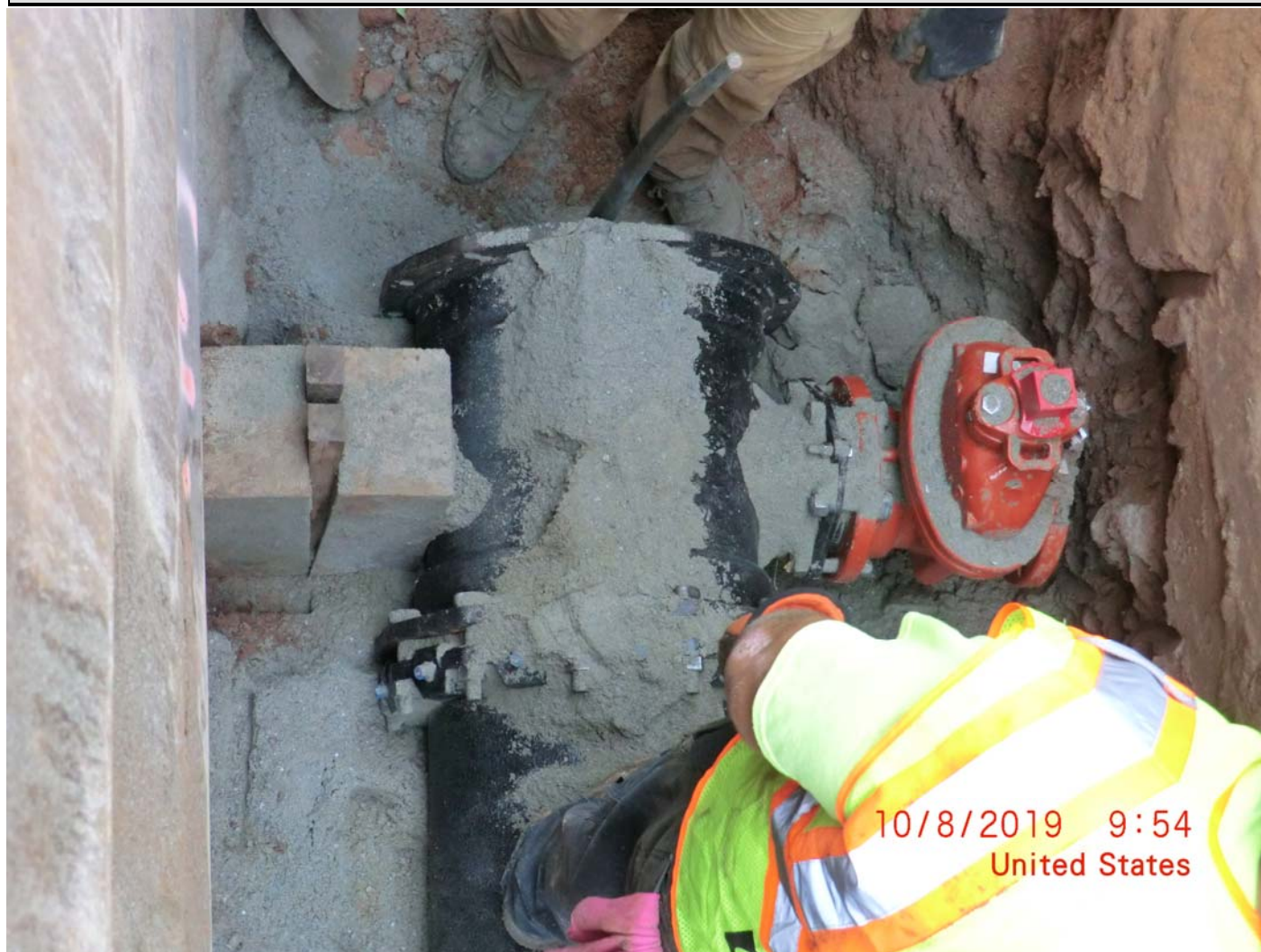
Figure 6 – Typical pipe seating. 16” DI Pipe to left - pipe bell to right (not shown in picture due to fill). Note the orange (top) and black (bottom) lock wedges seated in connection to bell.



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**Figure 7 – 16" DI pipe and fire hydrant Tee installed. 51+38, top of Tee is 6.08 feet below existing asphalt surface. Elevation of top of Tee at 349.7 feet. Remaining 6-foot section still to be installed.**

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Figure 8 – Installation of remaining 6 feet of 18-foot 16" DI pipe, with Megalug restraining gland.



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Figure 9 – Top of compacted 12-inch lift of pipe bedding. “Caution Water Line Below” tape on top.



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Figure 10 – Typical soil profile section located roughly at 51+00. From top, asphalt, road base, medium-fine sand, sandy silt (not shown – below pipe bedding backfill at bottom of picture).

<b>Res. Representative:</b>	W. Abrahams-Dematte	<b>Date:</b>	10/08/19
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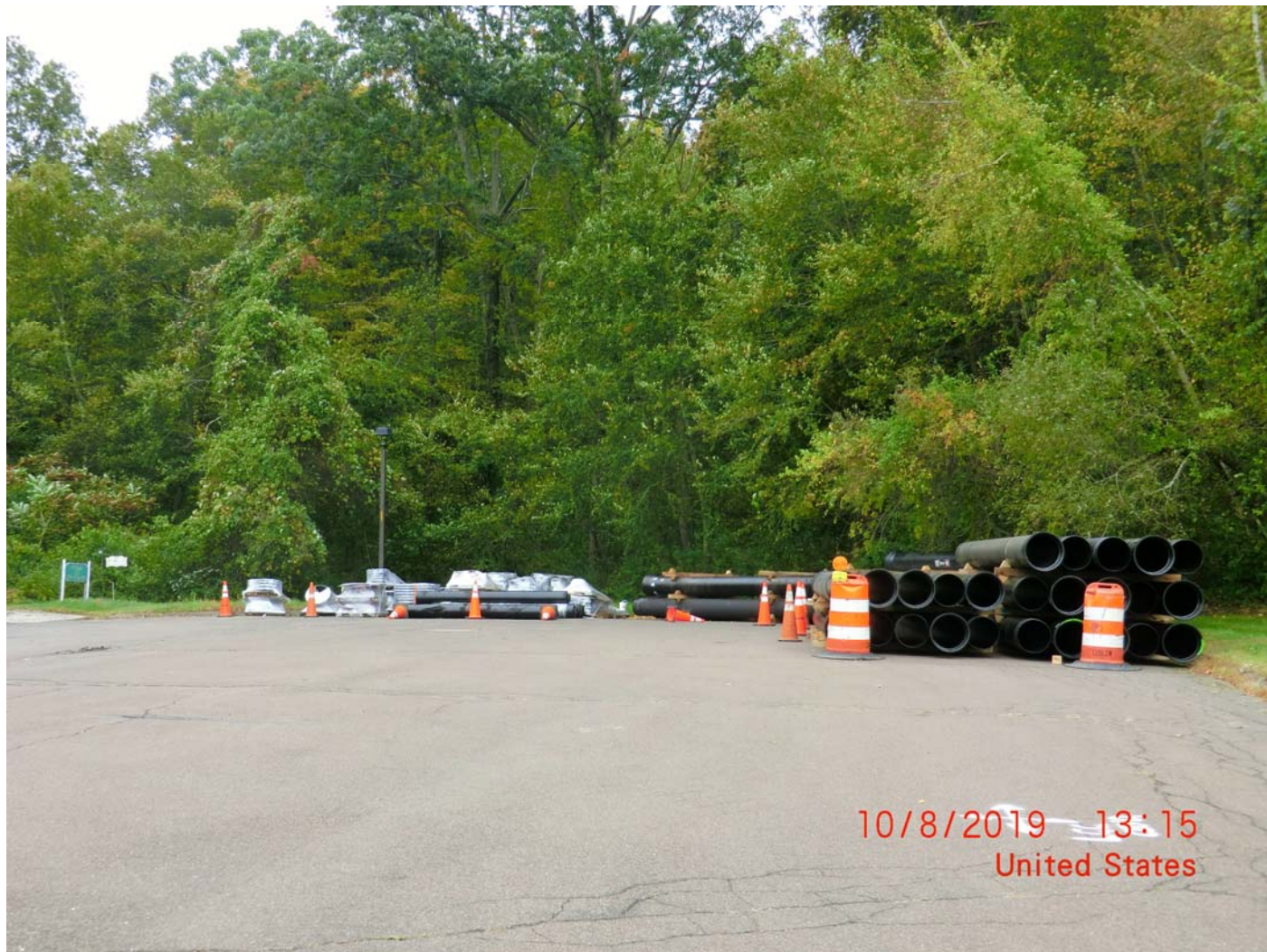


Figure 11 – Cul-de-sac, top of Talcott Ridge Drive. Staging area for pipe and materials.