



**US Army Corps
of Engineers**®
New England District
696 Virginia Road
Concord, MA 01742-2751

PUBLIC NOTICE

Comment Period Begins: 9/22/20
Comment Period Ends: 10/22/20
File Number: NAE-2020-01388
In Reply Refer To: Michael S. Wierbonics
Phone: (978) 318-8723
E-mail: Michael.S.Wierbonics@usace.army.mil

The District Engineer has received a permit application to conduct work in waters of the United States from Quonset Development Corporation. This work is proposed in Narragansett Bay at the seaward end of MacNaught Street, North Kingstown, Rhode Island. The site coordinates are: Latitude 41.584929, Longitude -71.420113.

Beginning inland and working seaward, the work includes the installation of a bulkhead, pile-supported pier with a catwalk and dolphin system. Removal of existing riprap along 183-linear-feet of shoreline generally within the footprint of the new pier. The existing riprap extends approximately 17 feet seaward from the sheetpile and the proposed riprap would extend out approximately 19.5 feet from the new sheetpile equating to an area of 2,288 SF of new riprap fill. The installation of 161-linear-feet of steel bulkhead, with approximately 62-linear-feet to run the full width of the new pier and the rest will extend approximately 50-feet on either side. The new bulkhead will be installed approximately 14-inches seaward of the outermost edge of the existing bulkhead with a section depth of 18-inches for a total distance of 2 feet 8 inches. Approximately 118-cubic-yards of fill material sourced from salvaged riprap from the existing revetment would be placed as backfill to stabilize the new bulkhead. Approximately 51 CY of the total volume of backfill will be discharged below the High Tide Line (HTL) between the existing bulkhead and the new bulkhead.

The pier will consist of a 62-foot-wide by 102-foot-long trestle supported by sixty (60) 30-inch-diameter pipe piles with HDPE sleeves running from Mean Low Water (MLW) to the bottom of the concrete cap. Twelve (12) of the total piles supporting the trestle will be located inshore. Located seaward and facing perpendicular of the trestle will be a 15-foot-wide by 120-foot-long sill supported by thirty (30) 30-inch-diameter pipe piles with HDPE sleeves running from Mean Low Water (MLW) to the bottom of the concrete cap. The deck will cover approximately 7,070 square feet of Narragansett Bay and the seventy-eight (78) pipe piles will impact 383 square feet of the bottom.

Seaward of the sill will be two (2) mooring dolphins protected by a fender. The dolphin system will have three (3) 4-foot-wide by 100-foot-long catwalks supported by two (2) 20-inch-diameter pipe piles. The monopole dolphins will consist of a 16-foot-wide by 16-foot-long reinforced concrete cap supported by a single 96-inch-diameter 1-inch-thick coated steel pipe pile. The fender unit will be mounted directly on the eastern side of the monopole.

The proposed permanent structures seaward of the existing bulkhead include the pier (7,070 sq. ft.), the sheet pile bulkhead (670 square feet), rip-rap (2,288 square feet) and the catwalk and dolphin system (1,670 square feet), with a total of 11,698 square feet.

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Depending on the location, some work will be performed by land using a controlled route, Burlington Avenue, while the more seaward work will be completed from barge mounted equipment. Piles and pile caps at the pier extension will be installed using a barge mounted crane. Piles will be driven using a combination of vibratory and impact hammering. Concrete caps will be poured by crew from barges and work floats.

The work is shown on the enclosed plans entitled “QUONSET DEVELOPMENT CORPORATION” on 18 sheets, and dated “April 17, 2020”.

The removal of existing rip-rap will offset some of the permanent impacts associated with the construction of the bulkhead, pier, catwalk and dolphin structures.

AUTHORITY

Permits are required pursuant to:

- Section 10 of the Rivers and Harbors Act of 1899
- Section 404 of the Clean Water Act
- Section 103 of the Marine Protection, Research and Sanctuaries Act.

The decision whether to issue a permit will be based on an evaluation of the probable impact of the proposed activity on the public interest. That decision will reflect the national concern for both protection and utilization of important resources. The benefit which may reasonably accrue from the proposal must be balanced against its reasonably foreseeable detriments. All factors which may be relevant to the proposal will be considered, including the cumulative effects thereof; among those are: conservation, economics, aesthetics, general environmental concerns, wetlands, cultural value, fish and wildlife values, flood hazards, flood plain value, land use, navigation, shoreline erosion and accretion, recreation, water supply and conservation, water quality, energy needs, safety, food production and, in general, the needs and welfare of the people.

The U.S. Army Corps of Engineers, New England District (Corps), is soliciting comments from the public; Federal, state, and local agencies and officials; Indian Tribes; and other interested parties in order to consider and evaluate the impacts of this proposed activity. The Corps will consider all comments received to determine whether to issue, modify, condition or deny a permit for this proposal. To make this decision, comments are used to assess impacts on endangered species, historic properties, water quality, general environmental effects, and the other public interest factors listed above. Comments are used in the preparation of an Environmental Assessment and/or an Environmental Impact Statement pursuant to the National Environmental Policy Act. Comments are also used to determine the need for a public hearing and to determine the overall public interest of the proposed activity.

Where the activity involves the discharge of dredged or fill material into waters of the United States or the transportation of dredged material for the purpose of disposing it in ocean waters, the evaluation of the impact of the activity in the public interest will also include application of the guidelines promulgated by the Administrator, U.S Environmental Protection Agency, under authority of Section 404(b) of the Clean Water Act, and/or Section 103 of the Marine Protection Research and Sanctuaries Act of 1972, as amended.

ESSENTIAL FISH HABITAT

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The Magnuson-Stevens Fishery Conservation and Management Act, as amended by the Sustainable Fisheries Act of 1996 (Public Law 104-267), requires all federal agencies to consult with the National Marine Fisheries Service on all actions, or proposed actions, permitted, funded, or undertaken by the agency, that may adversely affect Essential Fish Habitat (EFH). Essential Fish Habitat describes waters and substrate necessary for fish for spawning, breeding, feeding or growth to maturity.

This project will impact 0.27 of EFH. This habitat consists of organic silt and sand with organic fine sediments. Loss of this habitat may adversely affect species that use these waters and substrate. However, the District Engineer has made a preliminary determination that the site-specific adverse effect will not be substantial. Further consultation with the National Marine Fisheries Service regarding EFH conservation recommendations is being conducted and will be concluded prior to the final decision.

NATIONAL HISTORIC PRESERVATION ACT

Based on his initial review, the District Engineer has determined that little likelihood exists for the proposed work to impinge upon properties with cultural or Native American significance, or listed in, or eligible for listing in, the National Register of Historic Places. Therefore, no further consideration of the requirements of Section 106 of the National Historic Preservation Act of 1966, as amended, is necessary. This determination is based upon one or more of the following:

- a. The permit area has been extensively modified by previous work.
- b. The permit area has been recently created.
- c. The proposed activity is of limited nature and scope.
- d. Review of the latest published version of the National Register shows that no presence of registered properties listed as being eligible for inclusion therein are in the permit area or general vicinity.
- e. Coordination with the State Historic Preservation Officer and/or Tribal Historic Preservation Officer(s).

ENDANGERED SPECIES CONSULTATION

The Corps has reviewed the application for the potential impact on Federally-listed threatened or endangered species and their designated critical habitat pursuant to section 7 of the Endangered Species Act as amended. It is our preliminary determination that the proposed activity for which authorization is being sought is designed, situated or will be operated/used in such a manner that it is not likely to adversely affect a listed species or their critical habitat. We are coordinating with the National Marine Fisheries Service and/or U.S. Fish and Wildlife Service on listed species under their jurisdiction and the ESA consultation will be concluded prior to the final decision.

OTHER GOVERNMENT AUTHORIZATIONS

The states of Connecticut, Maine, Massachusetts, New Hampshire and Rhode Island have approved Coastal Zone Management Programs. Where applicable, the applicant states that any proposed activity will comply with and will be conducted in a manner that is consistent with the approved Coastal Zone Management Program. By this Public Notice, we are requesting the State concurrence or objection to the applicant's consistency statement.

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The following authorizations have been applied for, or have been, or will be obtained:

- (X) Permit, license or assent from State.
- (X) Permit from local wetland agency or conservation commission.
- (X) Water Quality Certification in accordance with Section 401 of the Clean Water Act.

COMMENTS

In order to properly evaluate the proposal, we are seeking public comment. Anyone wishing to comment is encouraged to do so. Comments should be submitted in writing by the above date. If you have any questions, please contact Michael S. Wierbonics at (978) 318-8723, (800) 343-4789 or (800) 362-4367, if calling from within Massachusetts.

Any person may request, in writing, within the comment period specified in this notice, that a public hearing be held to consider the application. Requests for a public hearing shall specifically state the reasons for holding a public hearing. The Corps holds public hearings for the purpose of obtaining public comments when that is the best means for understanding a wide variety of concerns from a diverse segment of the public.

The initial determinations made herein will be reviewed in light of facts submitted in response to this notice. All comments will be considered a matter of public record. Copies of letters of objection will be forwarded to the applicant who will normally be requested to contact objectors directly in an effort to reach an understanding.

THIS NOTICE IS NOT AN AUTHORIZATION TO DO ANY WORK.

Kevin R. Kotelly, P.E.
Chief, Permits and Enforcement Branch
Regulatory Division

If you would prefer not to continue receiving Public Notices by email, please contact Ms. Leslie Martin at (978) 318-8688 or e-mail her at leslie.martin@usace.army.mil. You may also check here () and return this portion of the Public Notice to: Leslie Martin, Regulatory Division, U.S. Army Corps of Engineers, 696 Virginia Road, Concord, MA 01742-2751.

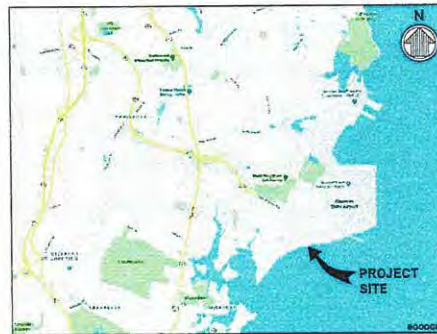
NAME: _____
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ELECTRIC BOAT OCEAN TRANSPORT BARGE PIER

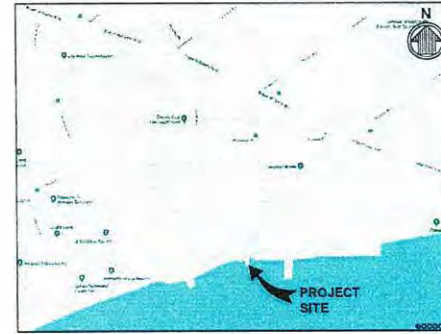
NORTH KINGSTOWN, RHODE ISLAND
 CONTRACT NO: 2020-020

**ISSUE FOR PERMITTING
 NOT FOR CONSTRUCTION
 APRIL 17, 2020**

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VICINITY MAP



LOCATION MAP



Development Services
 93 Crisp Street
 North Kingstown, RI 02852
 Tel: (401) 295-0044
 Fax: (401) 268-9885



180 WELLS AVENUE, SUITE 302
 NEWTON, MA 02459
 617-299-7330
 www.moffattnichol.com

**PERMIT SUBMITTAL
 ISSUED: 2020-04-17
 NOT TO BE USED FOR CONSTRUCTION**

| NO. | REVISION | DATE | APP. |
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| DESIGN BY: JAB | CHECKED BY: PRG |
| DRAWN BY: RDM | ENGINEER: JSC |
| SCALE: NOTED | PROJECT NO. 10763 |
| CONTRACT NO. . | |
| FILE NAME: 10763-01-0-01-0003 | |

64832030

DRAWING TITLE

COVER SHEET

Quonset Business Park,

SHEET NO. **G-001** DRAWING NO. **01**
 1 OF 18 SHEETS

GENERAL NOTES

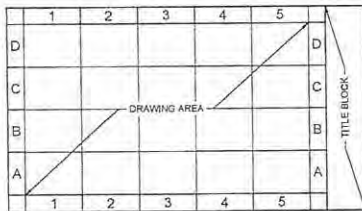
- GENERAL NOTES ARE NOT INTENDED TO REPLACE THE CONTRACT DOCUMENTS. SEE CONTRACT DOCUMENTS FOR REQUIREMENTS IN ADDITION TO THESE GENERAL NOTES. THE CONTRACT DOCUMENTS SHALL CONSIST OF THE COMPLETE PROJECT SPECIFICATIONS AND WORKING DRAWINGS INCLUDING BUT NOT LIMITED TO GENERAL PROVISIONS, SPECIAL PROVISIONS, DIVISION 1 REQUIREMENTS, TECHNICAL SPECIFICATIONS, AND ANY RELEVANT APPENDIX ITEMS. ALL WORK SHALL CONFORM TO THE REQUIREMENTS OF THE CONTRACT DOCUMENTS.
- THE WORKING DRAWINGS ARE NOT NECESSARILY COMPLETE IN EVERY DETAIL. THE CONTRACTOR SHALL PROVIDE ALL EQUIPMENT, MATERIAL, SERVICES, LABOR, ETC. FOR A COMPLETE INSTALLATION INCLUDING WORK REASONABLY INFERRED FROM THE CONTRACT DOCUMENTS AS BEING NECESSARY TO PRODUCE THE INTENDED RESULTS, WHETHER SHOWN OR NOT ON THE DRAWINGS.
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS BEFORE STARTING WORK. DO NOT SCALE PROJECT DRAWINGS. REPORT ANY DISCREPANCIES IN THE DRAWINGS AND/OR SPECIFICATIONS TO THE ENGINEER FOR CLARIFICATIONS OR ADJUSTMENTS PRIOR TO COMMENCING WORK. THE CONTRACTOR SHALL NOT BEGIN DEMOLITION/CONSTRUCTION IN ANY SUCH AFFECTED AREA UNTIL THE DISCREPANCY HAS BEEN RESOLVED.
- SHOULD THERE BE A CONFLICT BETWEEN THESE GENERAL NOTES WORKING DRAWINGS, AND/OR SPECIFICATIONS, THE MOST RESTRICTIVE INTERPRETATION SHALL PREVAIL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING FROM THE ENGINEER ANY CLARIFICATION OR INTERPRETATION OF THE GENERAL NOTES, WORKING DRAWINGS, AND/OR SPECIFICATIONS IN WRITING AND IN ADVANCE OF THE BEGINNING OF DEMOLITION/CONSTRUCTION. NUMERICAL DIMENSIONS AND ELEVATIONS SHOWN SHALL SUPERSEDE ANY DISCREPANCY IN THE SCALING ON THE DRAWINGS.
- ALL FEDERAL, STATE, AND LOCAL SAFETY REGULATIONS ARE TO BE STRICTLY FOLLOWED. METHODS OF DEMOLITION/CONSTRUCTION AND INSTALLATION OF MATERIAL IS THE CONTRACTOR'S RESPONSIBILITY.
- THE CONTRACTOR SHALL ABIDE BY ALL APPLICABLE FEDERAL, STATE, AND LOCAL ENVIRONMENTAL PROTECTION STANDARDS, LAWS, AND REGULATIONS.
- THE CONTRACTOR SHALL KEEP ACCURATE RECORDS OF ANY CHANGES MADE TO THE DRAWINGS ON A SEPARATE WRITE SET OF PLANS PROVIDED BY THE ENGINEER. THESE ANNOTATED DRAWINGS SHALL BE RETURNED TO THE ENGINEER PRIOR TO APPROVAL OF THE FINAL PAYMENT APPLICATION.
- UNLESS OTHERWISE NOTED, THE CONTRACTOR SHALL, ON A DAILY BASIS, REMOVE FROM THE SITE ANY DEBRIS RESULTING FROM DEMOLITION/CONSTRUCTION. DISPOSAL OF MATERIALS IS THE RESPONSIBILITY OF THE CONTRACTOR. ALL MATERIALS TO BE REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE NOTED, AND SHALL BE DISPOSED OF AS SPECIFIED. ALL DEBRIS SHALL BE PROPERLY DISPOSED OF IN A PERMITTED LANDFILL. THE CONTRACTOR SHALL KEEP RECORDS OF ALL MATERIALS REMOVED FROM THE SITE, INCLUDING DESCRIPTION, QUANTITIES, AND DISPOSAL LOCATION.
- EXISTING CONSTRUCTION, INCLUDING UTILITIES AND OTHER MISCELLANEOUS ITEMS WHICH ARE TO REMAIN, SHALL REMAIN UNDISTURBED AND BE PROTECTED, UNLESS NOTED OTHERWISE.
- THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING, AT HIS OWN EXPENSE, ANY AND ALL DAMAGES THAT MAY OCCUR OUTSIDE AND WITHIN THE LIMITS OF THIS PROJECT AS A RESULT OF DEMOLITION/CONSTRUCTION.
- ALL AREAS DISTURBED DURING DEMOLITION/CONSTRUCTION SHALL BE REPAIRED BY THE CONTRACTOR TO THEIR ORIGINAL CONDITION, AT NO EXPENSE TO THE OWNER, UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL PROTECT ADJACENT STRUCTURES, UTILITIES, PEDESTRIANS, VEHICULAR, AND MARINE TRAFFIC FROM POTENTIAL DAMAGE DUE TO CONTRACTOR'S OPERATIONS.
- THE CONTRACTOR SHALL PLACE CONSTRUCTION DEBRIS CONTROL DEVICES, TURBIDITY CURTAINS, BOOMS, TARPAILINGS, FLOATS, STAGING, AND OTHER DEVICES AS NECESSARY TO PREVENT CONSTRUCTION DEBRIS FROM ENTERING THE WATER AND AIRBORNE MATERIALS FROM LEAVING THE IMMEDIATE VICINITY OF THE SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANUP OF ANY MATERIALS DEPOSITED OUTSIDE THE WORK AREA. ALL WORK SHALL BE CONDUCTED IN ACCORDANCE WITH USACE PERMITS.
- ALL BUILDINGS, SURFACE, AND SUBSURFACE IMPROVEMENTS ON AND ADJACENT TO THE PROJECT SITE ARE NOT NECESSARILY SHOWN HEREON.
- THE OWNER SHALL HAVE THE SOLE AUTHORITY TO DESIGNATE AND/OR LIMIT AREAS OF CONSTRUCTION, STAGING, ACCESS, AND STORAGE.
- THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES SHOWN IN THE DRAWINGS ARE APPROXIMATE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE EXACT LOCATION OF ALL EXISTING UNDERGROUND UTILITIES BEFORE COMMENCING ANY WORK. THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF DAMAGES THAT OCCUR AS A RESULT OF A FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES.
- THE CONTRACTOR SHALL STAKE OUT ALL BASELINES OF CONSTRUCTION, THE LOCATION OF ALL NEW CONSTRUCTION, AND VERIFY ALL SETBACKS, OFFSETS, AND CLEARANCES PRIOR TO THE START OF WORK.
- WHERE PEDESTRIAN AND DRIVER SAFETY IS ENDANGERED IN THE AREA OF DEMOLITION/CONSTRUCTION WORK, USE TRAFFIC BARRICADES (JERSEY TYPE BARRIERS) WITH FLASHING LIGHTS. BARRICADES SHALL BE POSITIONED A MINIMUM OF 5 FEET FROM THE EDGE OF ANY OPENINGS IN THE STRUCTURE RESULTING FROM DEMOLITION/CONSTRUCTION ACTIVITIES.
- THE CONTRACTOR SHALL NOT OVERLOAD THE EXISTING STRUCTURE DURING DEMOLITION AND CONSTRUCTION OPERATION. OVERLOADING OR STORAGE MATERIALS WHICH WOULD RESULT IN OVERLOAD WILL NOT BE PERMITTED. CRANE OUTRIGGERS AND OTHER SUCH CONCENTRATED LOADS SHALL BE PROVIDED WITH CRSSING TO PROPERLY DISTRIBUTE THE LOAD. SAFETY OF PERSONNEL, REQUIRED EQUIPMENT, CONDITION, AND SUITABILITY OF THE EXISTING STRUCTURE TO SUPPORT MATERIAL AND EQUIPMENT LOADS IS THE CONTRACTOR'S RESPONSIBILITY.

GENERAL NOTES (CONTINUED)

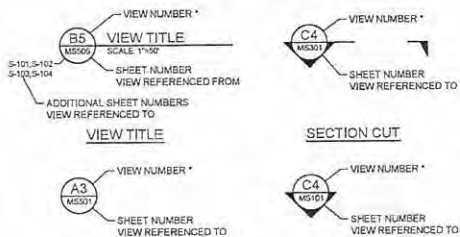
- CONSTRUCTION LOADS GREATER THAN 50% OF THE DESIGN LOADS FOR ANY NEW PORTION OF THE STRUCTURE SHALL NOT BE IMPOSED UNTIL THE CONCRETE CYLINDER STRENGTH FOR THOSE MEMBERS AND CONNECTING JOINTS HAVE REACHED 80% OF THE 28 DAY CONCRETE STRENGTH.
- FILES THAT BECOME DAMAGED OR FOR OTHER REASONS DO NOT BECOME A PERMANENT PART OF THE STRUCTURE SHALL BE EXTENDED.
- THE OWNER MAKES NO REPRESENTATIONS ABOUT SUBSURFACE CONDITIONS THAT MAY BE ENCOUNTERED WITHIN THE LIMITS OF THE PROJECT.
- A GEOTECHNICAL INVESTIGATION OF THE SITE WAS PERFORMED BY SCHABEL ENGINEER. THE BORINGS LOGS DATED NOVEMBER 1989 CAN BE MADE AVAILABLE UPON REQUEST.
- ALL INFORMATION IS PRESENTED TO BE AN ACCURATE REPRESENTATION OF THE SITE CONDITIONS. THE CONTRACTOR SHALL VERIFY ALL EXISTING CONDITIONS AT THE SITE, INCLUDING ALL ELEVATIONS REFERENCED TO LOCAL DATUM. REPORT SIGNIFICANT DISCREPANCIES/DIFFERENCES TO OWNER OR OWNER'S REPRESENTATIVE.
- ALL ELEVATIONS PROVIDED ON THE DRAWINGS ARE REFERENCED TO LOCAL QUONSET VERTICAL DATUM (QVD).
- THE FOLLOWING DATA IS PROVIDED REGARDING TIDAL INFORMATION. TIDAL INFORMATION WAS OBTAINED FROM WATERWAY SURVEYS & ENGINEERING, LTD.

| | QVD | NAVD83 |
|--|-------|--------|
| MHHW (MEAN HIGHER HIGH WATER) | +4.93 | +1.80 |
| MHW (MEAN HIGH WATER) | +4.68 | +1.63 |
| NAVD83 (NORTH AMERICAN VERTICAL DATUM) | +3.05 | 0.00 |
| MLW (MEAN LOW WATER) | +0.87 | -2.18 |
| MLLW (MEAN LOWER LOW WATER) | +0.72 | -2.33 |

7. TIDAL DATA IS PER NOAA AVERAGES BASED ON 1983-2001 TIDAL EPOCH AND NOT GUARANTEED TO REPRESENT CONDITIONS WHICH MAY OCCUR DURING CONSTRUCTION. ACTUAL WATER LEVELS WILL VARY FROM LEVELS INDICATED. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN ESTIMATES OF WATER LEVELS WHICH MAY OCCUR DURING CONSTRUCTION. VARIATION OF TIDAL LEVELS FROM THOSE INDICATED OR CONTRACTOR'S ESTIMATION OF TIDAL LEVELS SHALL NOT BE CONSIDERED AS A CLAIM FOR ADDITIONAL COMPENSATION OR DELAY OF WORK.



DRAWING AREA COORDINATE SYSTEM (DACS)



DETAIL CALLOUT

ELEVATION VIEW

* VIEW NUMBER IS BASED ON THE DACS LOCATION OF THE LOWER LEFT EXTENTS OF THE VIEW ON THE REFERENCED SHEET, WHEN REFERENCING DRAWING INFORMATION BETWEEN SHEETS. BOTH THE VIEW AND SHEET NUMBERS MUST BE QUOTED TOGETHER - EITHER IN A CALLOUT FORMAT AS SHOWN ABOVE OR IN THE FORM:
 * VIEW NO./SHEET NO.: (C15-301)

ABBREVIATIONS

| | | | | | |
|---------|--|-----------------|---|------|----------------------|
| ACI | AMERICAN CONCRETE INSTITUTE | LG | LONG | LIG | UNDERGROUND |
| ADA | AMERICANS WITH DISABILITIES ACT | LP | LIGHT POLE | UV | ULTRAVIOLET |
| AISC | AMERICAN INSTITUTE OF STEEL CONSTRUCTION | LRFD | LOAD AND RESISTANCE FACTOR DESIGN | VAR | VARIABLE |
| ALUM | ALUMINUM | LS | LANDSIDE | VERT | VERTICAL |
| ANSI | AMERICAN NATIONAL STANDARDS INSTITUTE | LT | LEFT DR LONG TONS (2,240 LB) | VLF | VERTICAL LINEAR FEET |
| APPROX | APPROXIMATE | M | METER | W | WEST |
| ARCH | ARCHITECTURAL | MAX | MAXIMUM | W/O | WITHOUT |
| ASCE | AMERICAN SOCIETY OF CIVIL ENGINEERS | MECH | MECHANICAL | WP | WORK POINT |
| ASME | AMERICAN SOCIETY OF MECHANICAL ENGINEERS | MEP | MECHANICAL, ELECTRICAL, PLUMBING | WS | WATERSIDE |
| ASTM | AMERICAN SOCIETY FOR TESTING AND MATERIALS | MH | MANHOLE | W.T. | WALL THICKNESS |
| AWPA | AMERICAN WOOD PROTECTION ASSOCIATION | MH-W | MEAN HIGHER HIGH WATER | WWF | WELDED WIRE FABRIC |
| AWS | AMERICAN WELDING SOCIETY | MHW | MEAN HIGH WATER | W | WATERSIDE |
| BIDS | BUILDING | MILS | THOUSANDS OF AN INCH | % | PERCENT |
| BOTT | BOTTOM | MIN | MINIMUM | & | AND |
| B.O.C. | BOTTOM OF CONCRETE CAST-IN-PLACE | MISC | MISCELLANEOUS | @ | DIAMETER |
| CIP | CONSTRUCTION JOINT/CONTROL JOINT | MLW | MEAN LOWER LOW WATER | ° | DEGREES |
| CF | CUBIC FEET | MOD | MODIFIED | | |
| CKD | CHECKED | MOF | MODULAR OUTFITTING FACILITY | | |
| CL | CENTERLINE | MPH | MILES PER HOUR | | |
| CLF | CLEAR | MT | METRIC TON (2,208 LB) | | |
| CM | CORRUGATED METAL | N | NORTH | | |
| CMU | CONCRETE MASONRY UNIT | NAVD | NORTH AMERICAN VERTICAL DATUM | | |
| COMP | COMPOSITE | NFPA | NATIONAL FIRE PROTECTION ASSOCIATION | | |
| CONC | CONCRETE | NOT IN CONTRACT | | | |
| CONST | CONSTRUCTION | NOA | NORTH ATLANTIC OCEANIC AND NATIONAL OCEAN SERVICE | | |
| CONT | CONTINUOUS | NOG | NORTH OCEAN SERVICE | | |
| CONT | CONTINUED | NTS | NOT TO SCALE | | |
| CORR | CORRUGATED | D.C. | ON CENTER | | |
| CSK | COUNTERSINK | O.D. | OUTSIDE DIAMETER | | |
| CTS | CENTERS | OTB | OCEAN TRANSPORT BARGE | | |
| COVER | COVER | PC | PRECAST | | |
| CY | CUBIC YARDS | PCF | POUNDS PER CUBIC FOOT | | |
| DACS | DRAWING AREA COORDINATE SYSTEM | PCI | PRECAST/PRESTRESSED CONCRETE INSTITUTE | | |
| DBL | DOUBLE | PDA | PILE DYNAMIC ANALYZER | | |
| DEG | DEGREE | PEN | PENETRATION | | |
| DEMO | DEMOLITION | PI | POINT OF INTERSECTION | | |
| DET | DETAIL | PT | PRESSURE TREATED | | |
| DFT | DRY FILM THICKNESS | PERF | PERFORATED | | |
| DIA | DIAMETER | PL | PLATE | | |
| DI | DUCTILE IRON | PROP | PROPOSED | | |
| DISCONT | DISCONTINUOUS | PSF | POUNDS PER SQUARE FOOT | | |
| DN | DOWN | PGB | POUNDS PER SQUARE INCH | | |
| DOCS | DOCUMENTS | PVC | POLYVINYL CHLORIDE | | |
| DWG | DRAWING | QTY | QUANTITY | | |
| DWN | DRAWN | R | RADIUS | | |
| E | EAST | R/C | REINFORCED CONCRETE | | |
| EA | EACH | R/F | REINFORCED | | |
| EF | EACH FACE | REINF | REINFORCED/REINFORCEMENT | | |
| EJ | EXPANSION JOINT | REV | REVISION | | |
| EJ/ELEV | ELEVATION | REQD | REQUIRED | | |
| ELEC | ELECTRICAL | RT | RIGHT | | |
| ELL | ELBOW | S | SOUTH | | |
| EMBED | EMBEDMENT | SCH | SCHEDULE | | |
| EPDM | ETHYLENE PROPYLENE DIENE MONOMER | SE | SOUTHEAST | | |
| ETC | ET CETERA | SEC | SECOND | | |
| EQ | EQUAL | SF | SQUARE FOOT | | |
| EW | EACH WAY | SHT | SHEET | | |
| EXIST. | EXISTING | SPA | SPACES | | |
| EXP | EXPANSION | SPMT | SELF-PROPELLED MOBILE TRANSPORTER | | |
| F | FAHRENHEIT | SQ | SQUARE | | |
| FDD | FLOATING DRY DOCK | SS | STAINLESS STEEL | | |
| FRP | FIBER REINFORCED POLYMER | SSP | STEEL SHEET PILE | | |
| FT | FEET | ST | STREET OR SHORT TON | | |
| GALV | GALVANIZED | STA | STATION | | |
| GR | GRADE | STD | STANDARD | | |
| HDPE | HIGH DENSITY POLYETHYLENE | STL | STEEL | | |
| HORIZ | HORIZONTAL | SW | SOUTHWEST | | |
| HSS | HOLLOW STRUCTURAL SECTION | T | TON (2,000 LB) | | |
| IBC | INTERNATIONAL BUILDING CODE | T&B | TOP & BOTTOM | | |
| ID | INTERNAL DIAMETER | TBD | TO BE DETERMINED | | |
| IN | INCHES | TBR | TO BE REMOVED | | |
| INC | INCORPORATED | TK | THICK | | |
| INFO | INFORMATION | TOC | TOP OF CONCRETE | | |
| INV | INVERT ELEVATION | TOS | TOP OF STEEL | | |
| JT | JOINT | TPY | TYPICAL | | |
| KIP | 1000 LB | LFC | UNIFIED FACILITIES CRITERIA | | |
| KSI | KIPS PER SQUARE INCH | LHMWPE | ULTRA HIGH MOLECULAR WEIGHT POLYETHYLENE | | |
| L | ANGLE OR LENGTH | LWC | UNIFIED NATIONAL COARSE | | |
| LB | POUND | UDN | UNLESS OTHERWISE NOTED | | |
| LF | LINEAR FEET | | | | |



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| DESIGN BY: JAB | CHECKED BY: PBC |
| DRAWN BY: RDM | ENGINEER: JSC |
| SCALE: NOTED | PROJECT NO.: 10763 |

CONTRACT NO.:

FILE NAME: 10763-01-G-002-DWG

04/05/2020

DRAWING TITLE

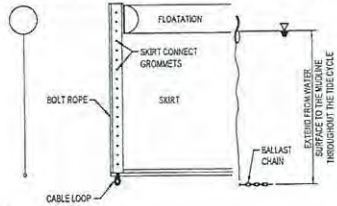
GENERAL NOTES & ABBREVIATIONS

Quonset Business Park,

SHEET NO. **G-002** DRAWING NO. **02**
 2 OF 18 SHEETS

TURBIDITY CURTAIN:

1. A FLOATING TURBIDITY BARRIER SHALL BE EMPLOYED AROUND AND/OR IMMEDIATELY ADJACENT TO THE WORK AREA DURING EACH CONSTRUCTION PHASE THAT IS EXPECTED TO PRODUCE DEBRIS AND/OR SEDIMENT (E.G. DEMOLITION OF EXISTING TRESTLES, AND INSTALLATIONS OF FENDER SYSTEMS, CONCRETE DECKCLUBS, CLEATS, AND BOLLARDS) IN 800 FOOT (MAX) LENGTHS. TURBIDITY CURTAINS ARE NOT EXPECTED TO BE REQUIRED DURING PILE DRIVING.



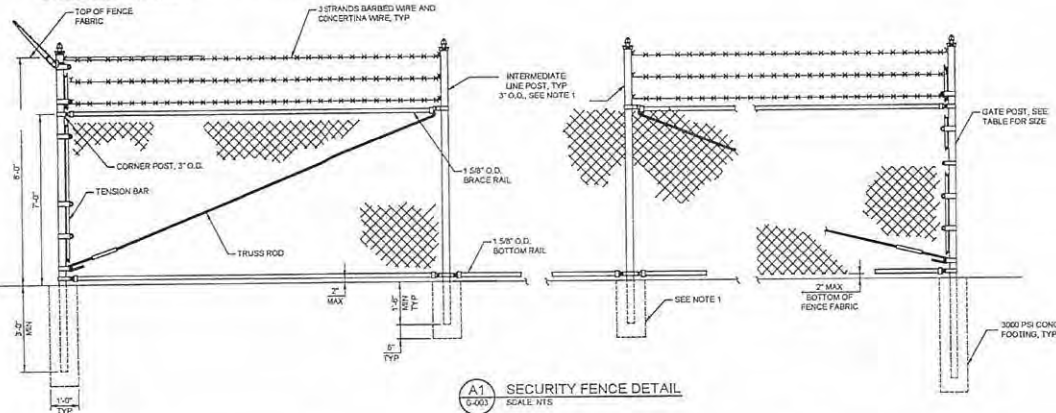
NOTES:

1. FIGURE IS FOR REFERENCE ONLY, TURBIDITY CURTAIN SHALL BE SUBMITTED FOR APPROVAL.
2. ANCHOR CURTAIN ENDS AT BULKHEAD FACE.
3. ANCHOR CURTAIN AT INTERMEDIATE POINTS WITH CONCRETE ANCHOR BOLTS OR SUITABLE ANCHORAGE ACCEPTED BY THE OWNER.

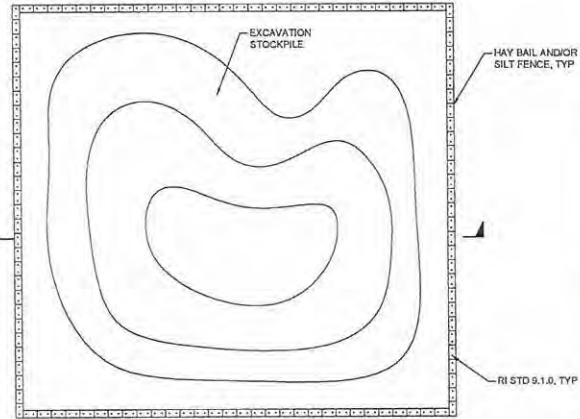
C1 TURBIDITY BARRIER DETAIL
SCALE: NTS

| GATE POSTS | | | |
|----------------|--------------|---------------------|--------------|
| PIPE SIZE | | SWING GATE OPENINGS | |
| NOM. O.D. PIPE | WEIGHT LB/FT | SINGLE GATE | DOUBLE GATE |
| 3.00" | 5.79 | 10" THRU 12" | 12" THRU 12" |
| 4.00" | 8.11 | 7" THRU 13" | 13" THRU 26" |
| 6.00" | 18.67 | 14" THRU 18" | 27" THRU 30" |

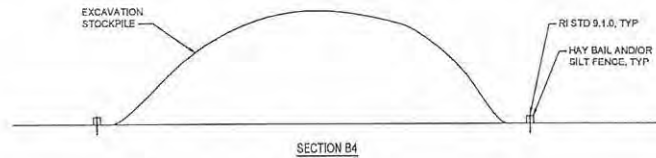
1. PIPE SECTIONS SHOWN ARE ASTM F1003 FOR STANDARD WEIGHT (SCHEDULE 40) PIPE. EQUIVALENT STEEL SECTIONS FOR FRAME SHALL BE BASED ON PIPE SECTION SHOWN (SEE SPECIFICATIONS).
2. WHEREVER POSSIBLE, AVOID LOCATING POSTS OVER UTILITY LINES BY ADJUSTING POST SPACING.
3. ALL EXPOSED METAL SHALL BE HOT-DIPPED GALVANIZED.
4. CHAIN LINK FABRIC SHALL BE 1.3m GALVANIZED COATED STEEL 2" DIAMOND MESH, 9 GAUGE WIRE WITH BARBED SELVAGES AT TOP AND BOTTOM.
5. GROUND FENCE AS RECOMMENDED BY MANUFACTURER.
6. FOR PERMANENT GATES IN SECURITY FENCE SEE SPECIFICATIONS REGARDING CONDUIT INSTALLATION.
7. FOR ADDITIONAL REQUIREMENTS FOR FENCE SEE SPECIFICATIONS.
8. CONTRACTOR TO PROVIDE SHOP DRAWING FOR LARGER SWING GATES, WITH WHEEL SUPPORTS FOR APPROVAL.
9. CONTRACTOR SHALL ENSURE A MAXIMUM GAP OF 2" BETWEEN FINAL GRADE AND BOTTOM OF FENCE FABRIC THROUGHOUT SITE.



A1 SECURITY FENCE DETAIL
SCALE: NTS



PLAN



SECTION B4

NOTES:

1. THE STOCKPILE SHOWN IS CONSIDERED TYPICAL AND MAY VARY.
2. SEE THE STOCKPILE MANAGEMENT NOTES FOR STOCKPILE REQUIREMENTS FOR VISIBLY AND/OR OLFACTORY CONTAMINATED SOIL.
3. SILT SOCKS MAY BE USED IN LIEU OF HAY BAILS AND/OR SILT FENCES AT THE CONTRACTOR'S DISCRETION.

B4 STOCKPILE DETAIL
SCALE: NTS



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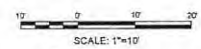
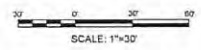
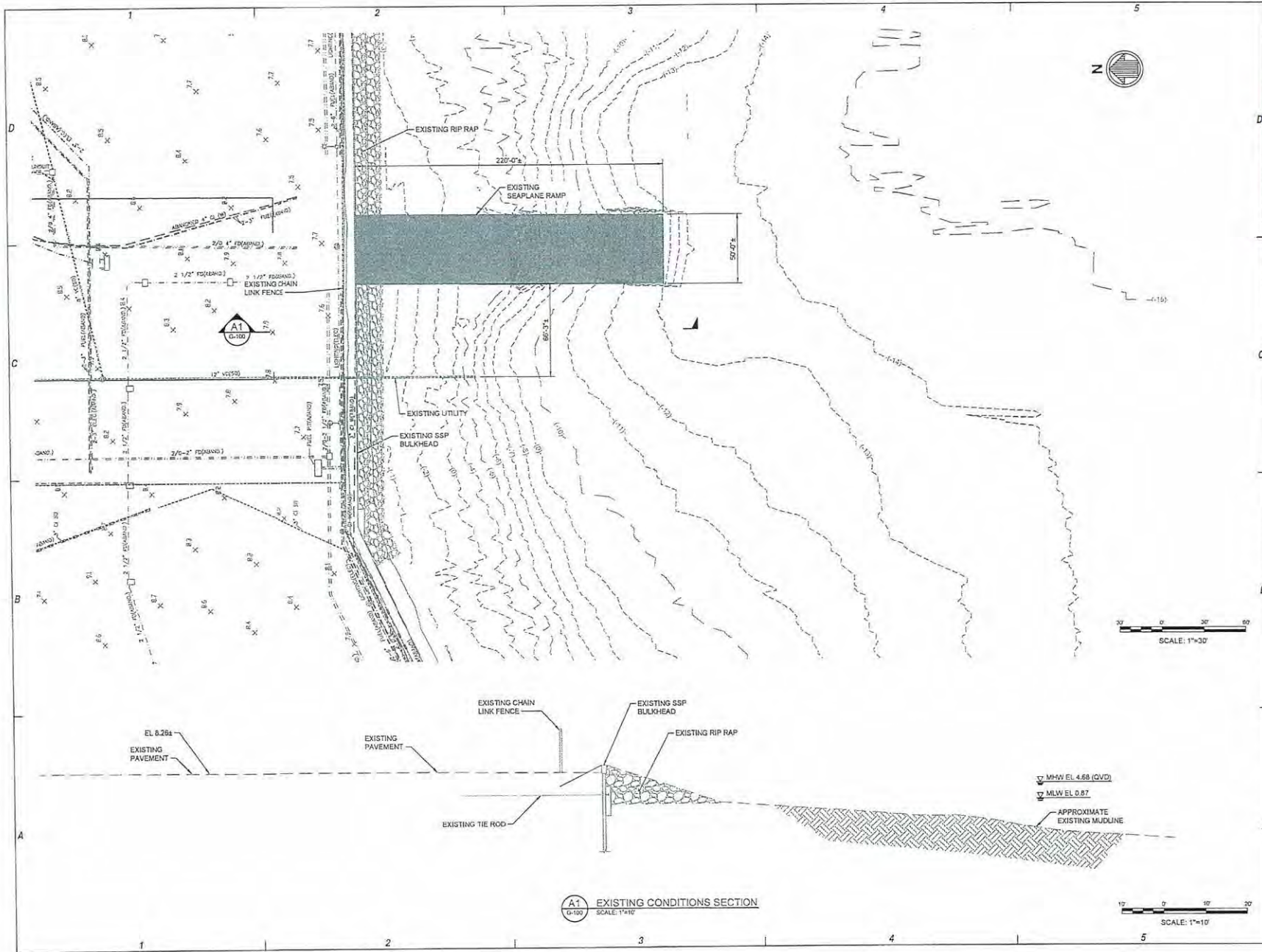
04/06/2020

DRAWING TITLE

GENERAL DETAILS

Quonset Business Park

SHEET NO. **G-003** DRAWING NO. **03**
3 OF 18 SHEETS



A1
G-100
EXISTING CONDITIONS SECTION
SCALE: 1"=10'



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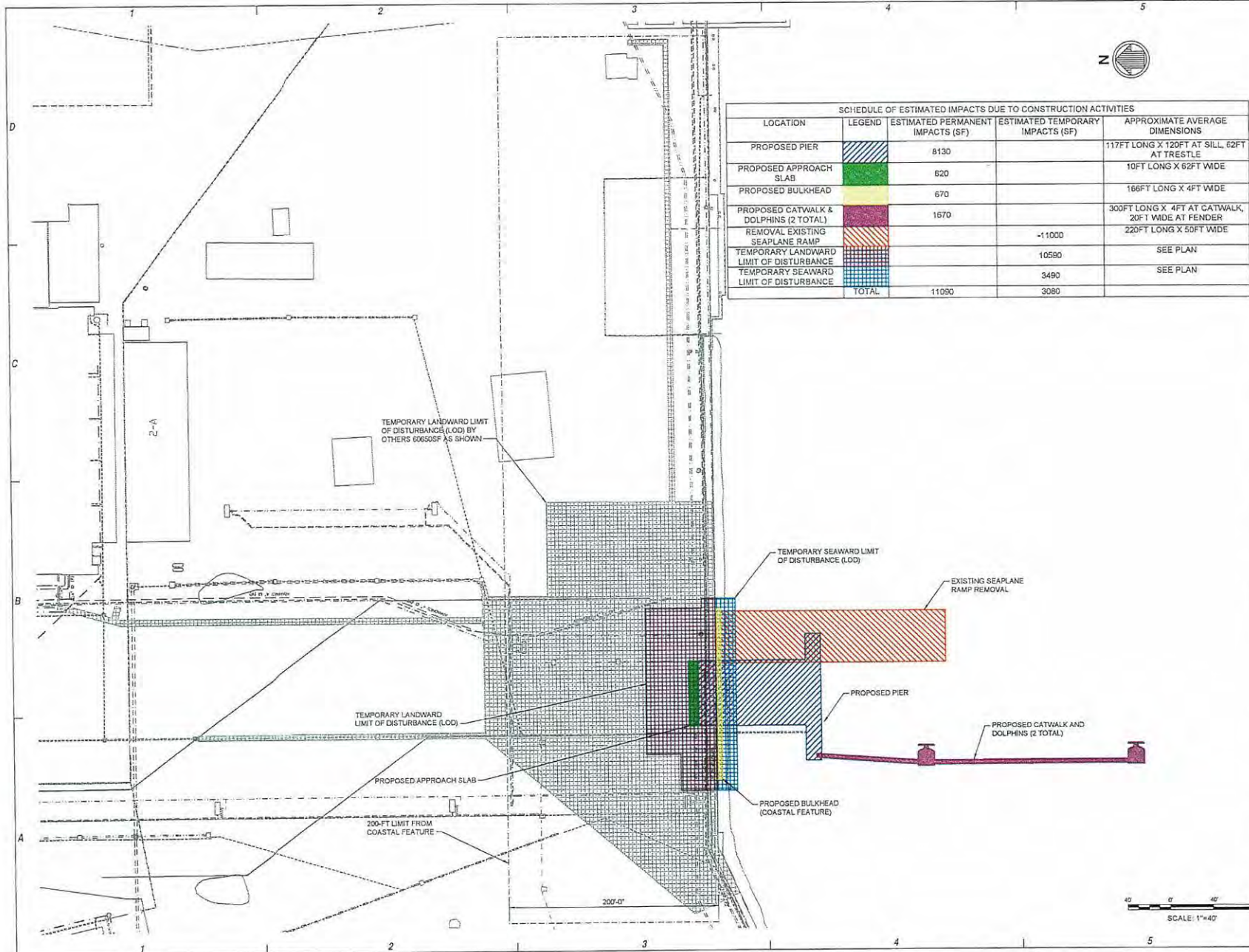
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| DRAWN BY: RDM | ENGINEER: JSC |
| SCALE: NOTED | PROJECT NO.: 10763 |
| CONTRACT NO.: | |
| FILE NAME: 10763-01-G-100.DWG | |

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DRAWING TITLE
**EXISTING CONDITION -
SITE PLAN AND
SECTION**

Quonset Business Park,

SHEET NO. DRAWING NO.
G-100 04
4 OF 18 SHEETS



| SCHEDULE OF ESTIMATED IMPACTS DUE TO CONSTRUCTION ACTIVITIES | | | | |
|--|--------|----------------------------------|----------------------------------|--|
| LOCATION | LEGEND | ESTIMATED PERMANENT IMPACTS (SF) | ESTIMATED TEMPORARY IMPACTS (SF) | APPROXIMATE AVERAGE DIMENSIONS |
| PROPOSED PIER | | 8130 | | 117FT LONG X 120FT AT SILL, 62FT AT TRESTLE |
| PROPOSED APPROACH SLAB | | 620 | | 10FT LONG X 62FT WIDE |
| PROPOSED BULKHEAD | | 670 | | 166FT LONG X 4FT WIDE |
| PROPOSED CATWALK & DOLPHINS (2 TOTAL) | | 1670 | | 300FT LONG X 4FT AT CATWALK, 20FT WIDE AT FENDER |
| REMOVAL EXISTING SEAPLANE RAMP | | | -11000 | 220FT LONG X 50FT WIDE |
| TEMPORARY LANDWARD LIMIT OF DISTURBANCE | | | 10580 | SEE PLAN |
| TEMPORARY SEAWARD LIMIT OF DISTURBANCE | | | 3490 | SEE PLAN |
| TOTAL | | 11080 | 3080 | |



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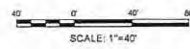
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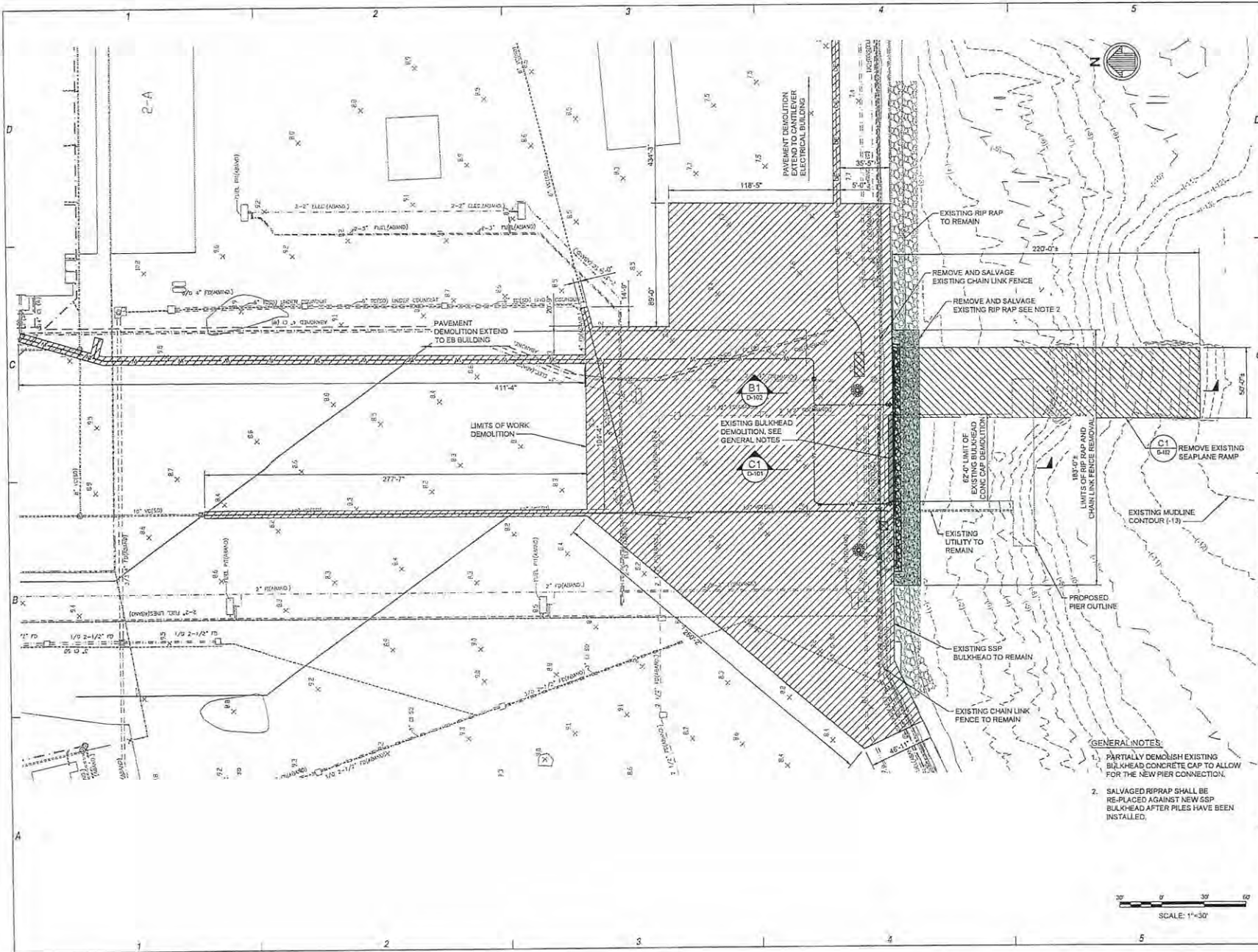
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| DRAWN BY: RDM | ENGINEER: JSC |
| SCALE: NOTED | PROJECT NO: 10763 |
| CONTRACT NO. | |
| FILE NAME: 10763-01-G-101.DWG | |

DRAWING TITLE
TEMPORARY AND PERMANENT IMPACTS DUE TO CONSTRUCTION

Quonset Business Park,
 SHEET NO. **G-101** 05
 5 OF 11 SHEETS





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| DRAWN BY: RDM | ENGINEER: JSC |
| SCALE: NOTED | PROJECT NO. 10763 |
| CONTRACT NO. : | |
| FILE NAME: 10763-05-100.DWG | |

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DRAWING TITLE

DEMOLITION PLAN VIEW

Quonset Business Park,

| | |
|---|--------------------------|
| SHEET NO. D-100 6 OF 18 SHEETS | DRAWING NO. 06 |
|---|--------------------------|

- GENERAL NOTES:**
- PARTIALLY DEMOLISH EXISTING BULKHEAD CONCRETE CAP TO ALLOW FOR THE NEW PIER CONNECTION.
 - SALVAGED RIPRAP SHALL BE RE-PLACED AGAINST NEW SSP BULKHEAD AFTER PILES HAVE BEEN INSTALLED.



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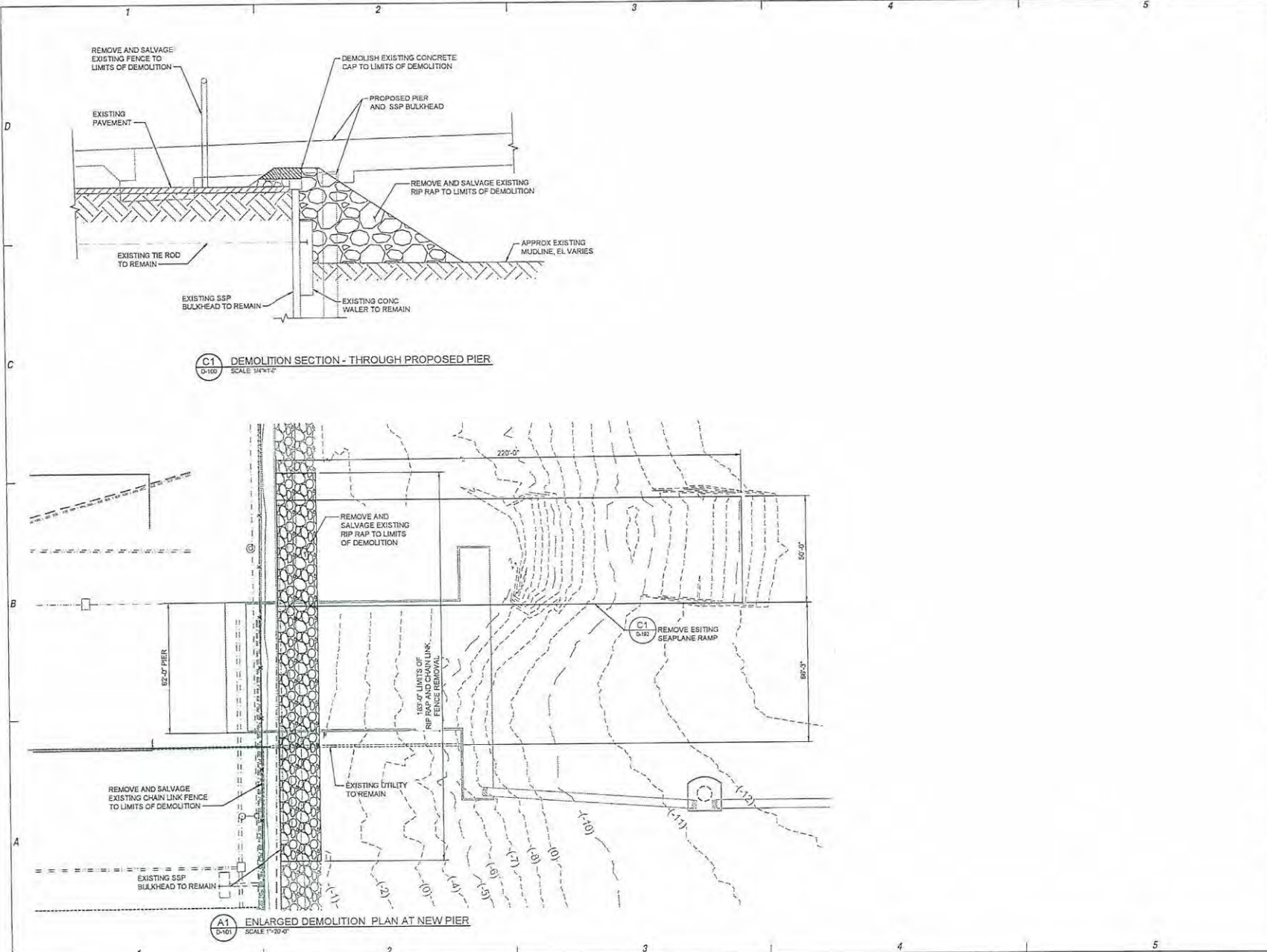
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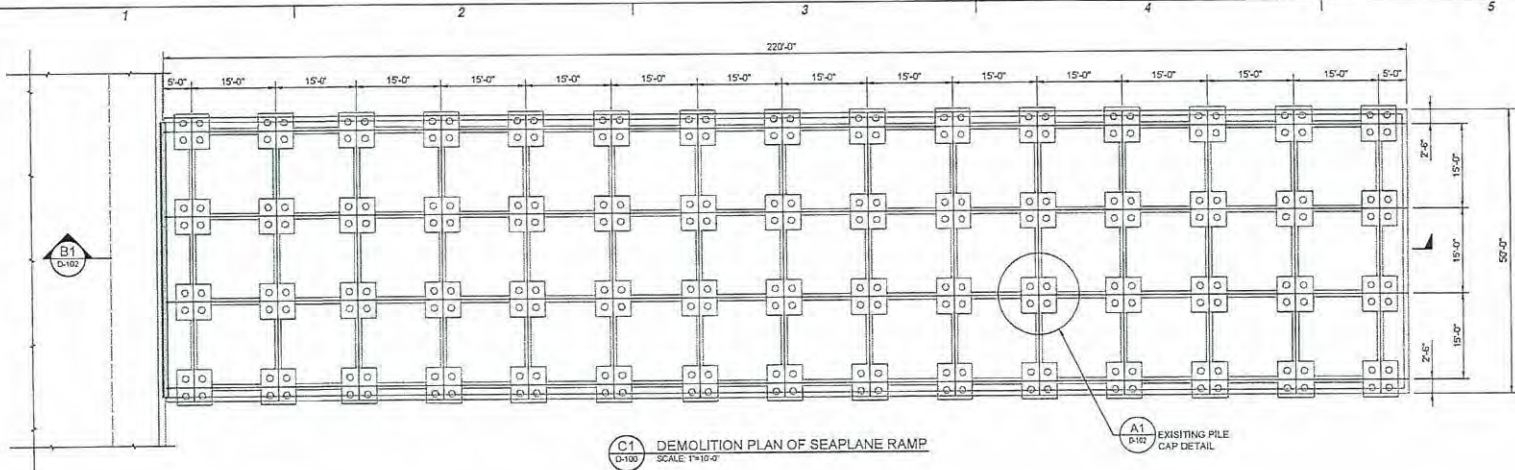
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| DRAWN BY: HDM | ENGINEER: JSC |
| SCALE: NOTED | PROJECT NO. 10763 |
| CONTRACT NO. _____ | |
| FILE NAME: 10763-01-0-101.DWG | |

DRAWING TITLE
DEMOLITION SECTION AND DETAILS
 Quonset Business Park,

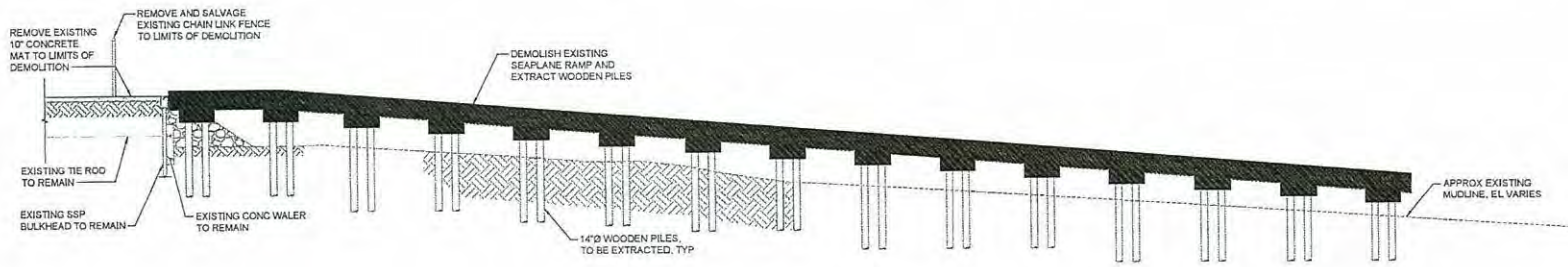
SHEET NO. **D-101**
 DRAWING NO. 07
 7 OF 16 SHEETS



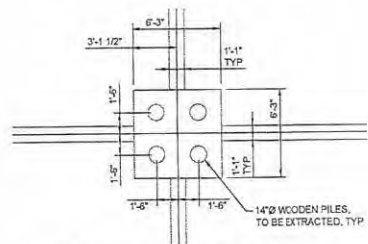


C1 DEMOLITION PLAN OF SEAPLANE RAMP
D-102 SCALE: 1"=10'-0"

A1 EXISTING PILE CAP DETAIL
D-102



B1 DEMOLITION SECTION - THROUGH SEAPLANE RAMP
D-102 SCALE: 1"=10'-0"



A1 DETAIL SEAPLANE PILE CAP
D-102 SCALE: 1/4"=1'-0"



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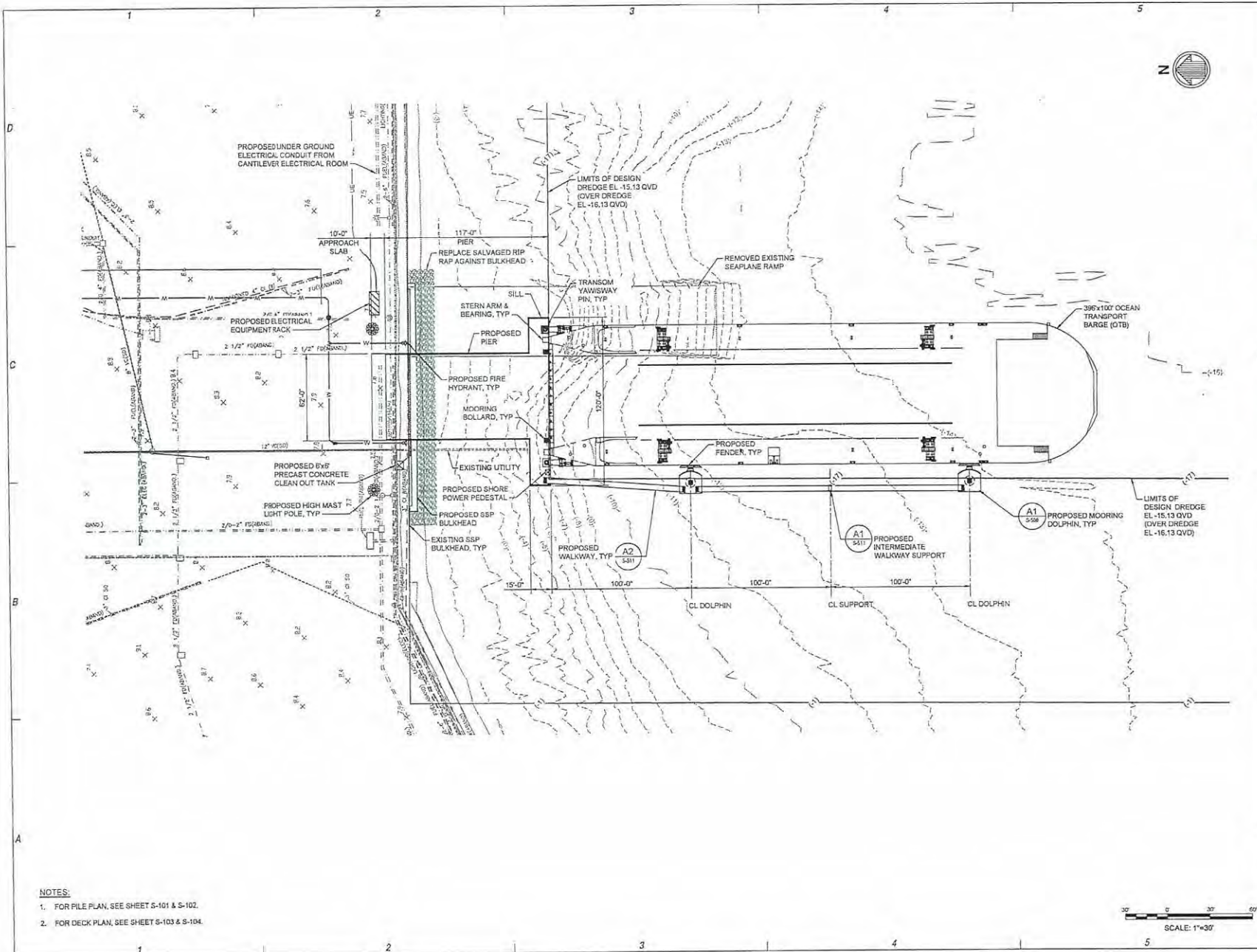
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| DRAWN BY: RDM | ENGINEER: JSC |
| SCALE: NOTED | PROJECT NO: 10763 |
| CONTRACT NO.: | |
| FILE NAME: 10763-010-102.DWG | |

04/14/2020
DRAWING TITLE
**DEMOLITION PLAN
SEAPLANE RAMP**
Quonset Business Park,

| | |
|---|--------------------------|
| SHEET NO. D-102 5 OF 18 SHEETS | DRAWING NO. 08 |
|---|--------------------------|



- NOTES:**
1. FOR FILE PLAN, SEE SHEET S-101 & S-102.
 2. FOR DECK PLAN, SEE SHEET S-103 & S-104.

SCALE: 1"=30'



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| SCALE: NOTED | PROJECT NO. 10763 |
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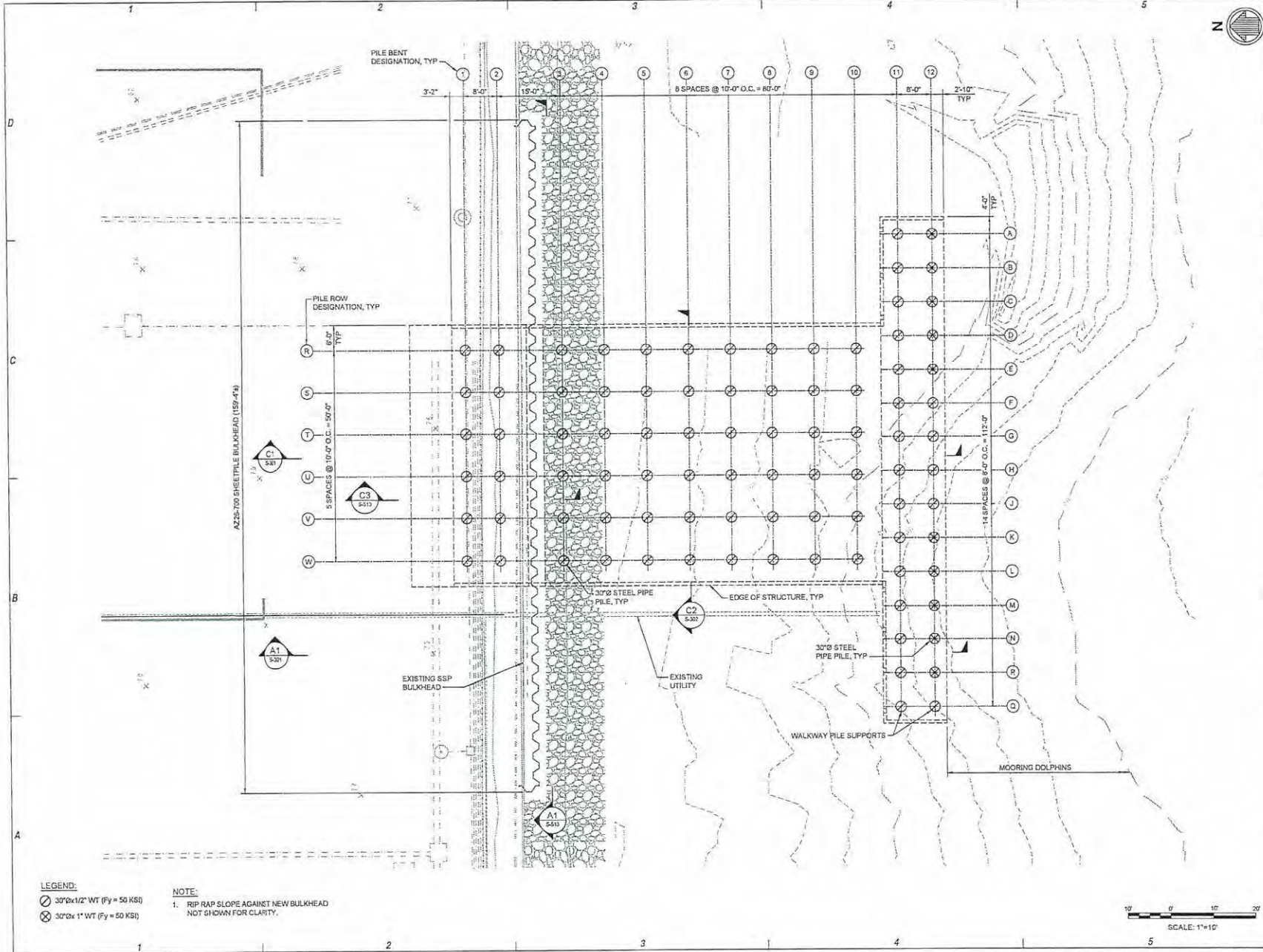
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DRAWING TITLE

OVERALL SITE PLAN

Quonset Business Park,

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| SHEET NO. | DRAWING NO. |
| S-100 | 09 |
| 3 OF 18 SHEETS | |



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| DRAWN BY: RDM | ENGINEER: JSC |
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| CONTRACT NO. : | |
| FILE NAME: 1076301-5-101.DWG | |

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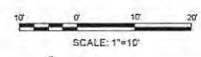
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PIER PILE LAYOUT

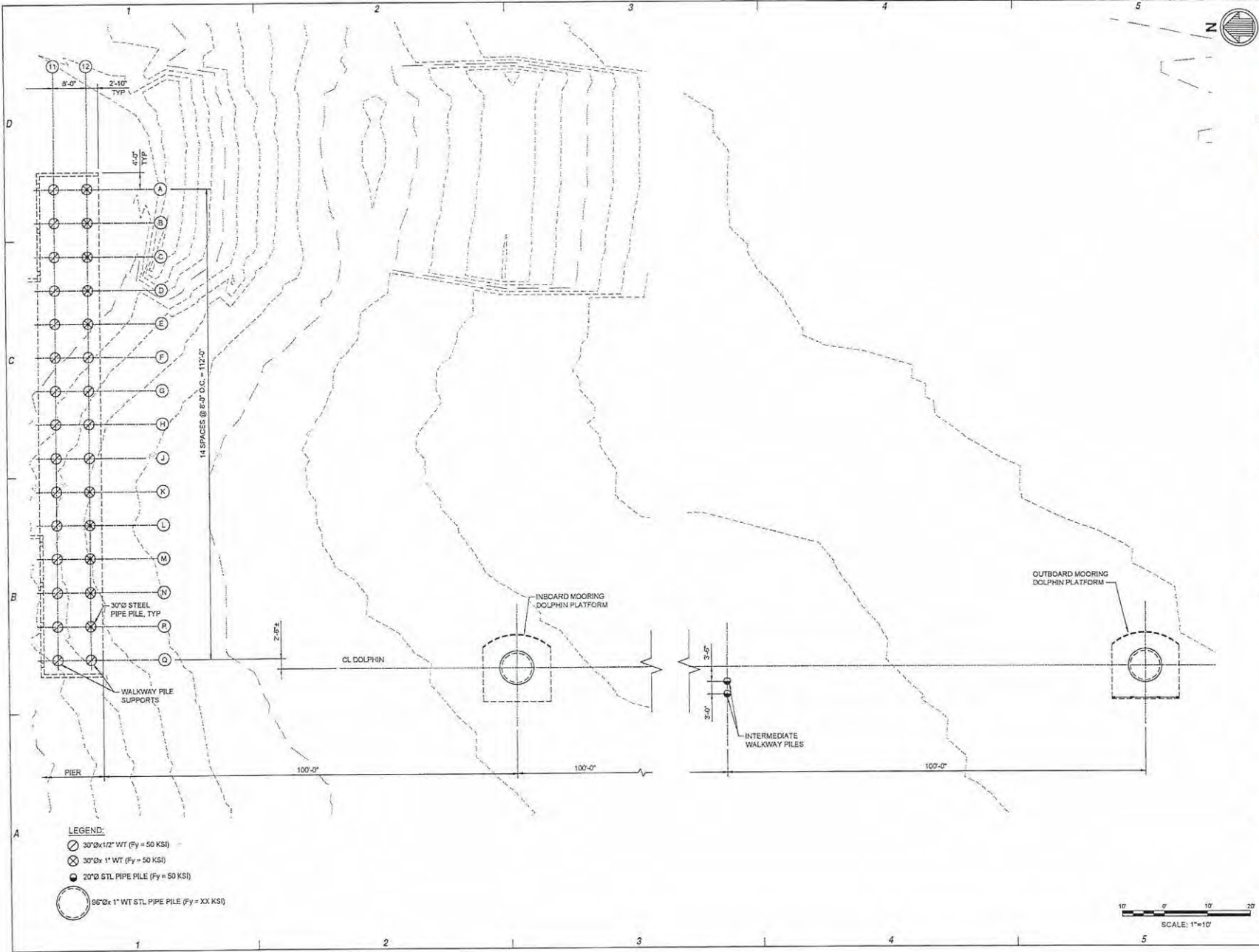
Quonset Business Park,

SHEET NO. **S-101** DRAWING NO. **10**
16 OF 18 SHEETS

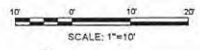
LEGEND:
 ⊗ 30"Øx1/2" WT (F_y = 50 KSI)
 ⊗ 30"Øx 1" WT (F_y = 50 KSI)

NOTE:
 1. RIP RAP SLOPE AGAINST NEW BULKHEAD NOT SHOWN FOR CLARITY.





- LEGEND:
- 30"Øx112" WT (Fy = 50 KSI)
 - ⊗ 30"Øx1" WT (Fy = 50 KSI)
 - 20"Ø STL PIPE PILE (Fy = 50 KSI)
 - 96"Øx1" WT STL PIPE PILE (Fy = XX KSI)



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| CONTRACT NO.: | | | |
| FILE NAME: | 10763-01-S-102.DWG | | |

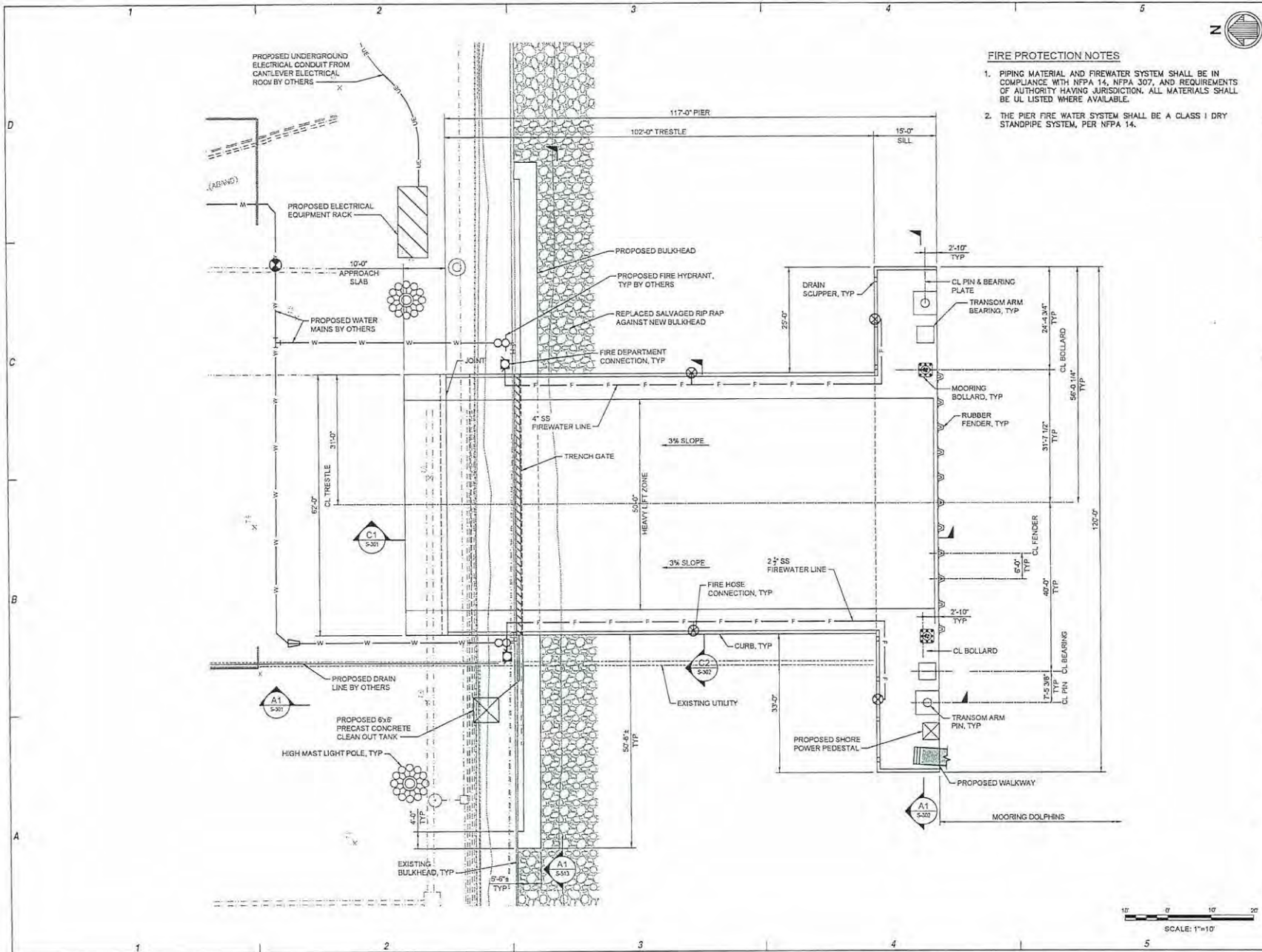
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**DOLPHIN AND
WALKWAY PILE
LAYOUT**

Quonset Business Park,

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| SHEET NO. S-102 11 OF 18 SHEETS | DRAWING NO. 11 |
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FIRE PROTECTION NOTES

1. PIPING MATERIAL AND FIREWATER SYSTEM SHALL BE IN COMPLIANCE WITH NFPA 14, NFPA 307, AND REQUIREMENTS OF AUTHORITY HAVING JURISDICTION. ALL MATERIALS SHALL BE UL LISTED WHERE AVAILABLE.
2. THE PIER FIRE WATER SYSTEM SHALL BE A CLASS I DRY STANDPIPE SYSTEM, PER NFPA 14.



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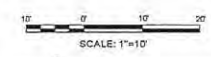
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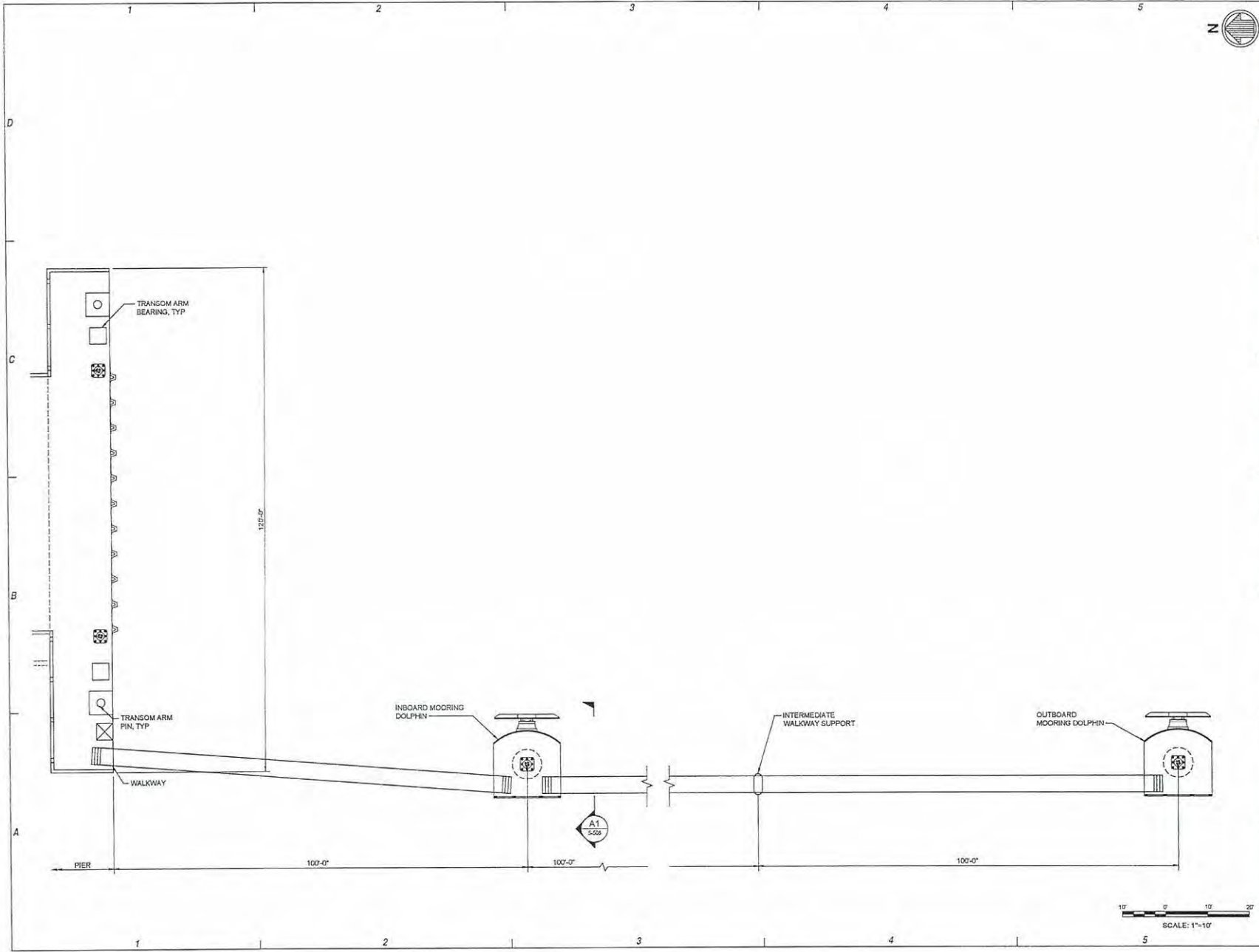
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 DRAWN BY: RDM ENGINEER: JSC
 SCALE: NOTED PROJECT NO. 10763
 CONTRACT NO. .
 FILE NAME: 10763-04-S-103.DWG

DRAWING TITLE
PIER DECK LAYOUT
 Quonset Business Park,

SHEET NO. **S-103** DRAWING NO. 12
 12 OF 15 SHEETS





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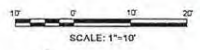
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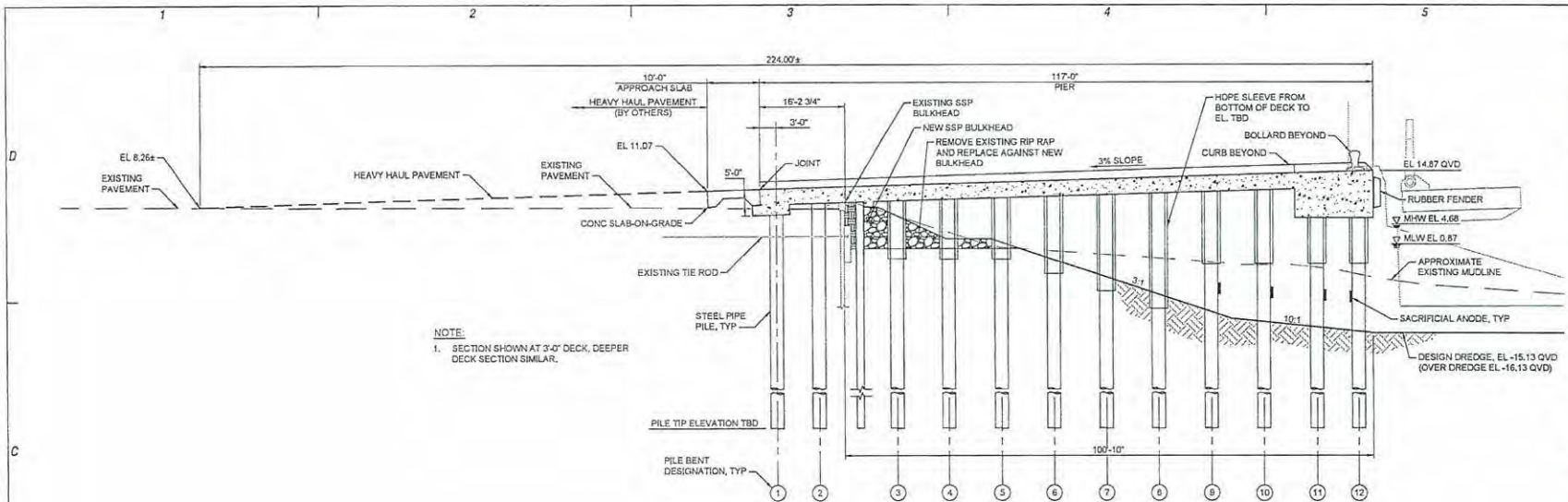
DRAWING TITLE

**MOORING DOLPHIN
AND WALKWAY LAYOUT**

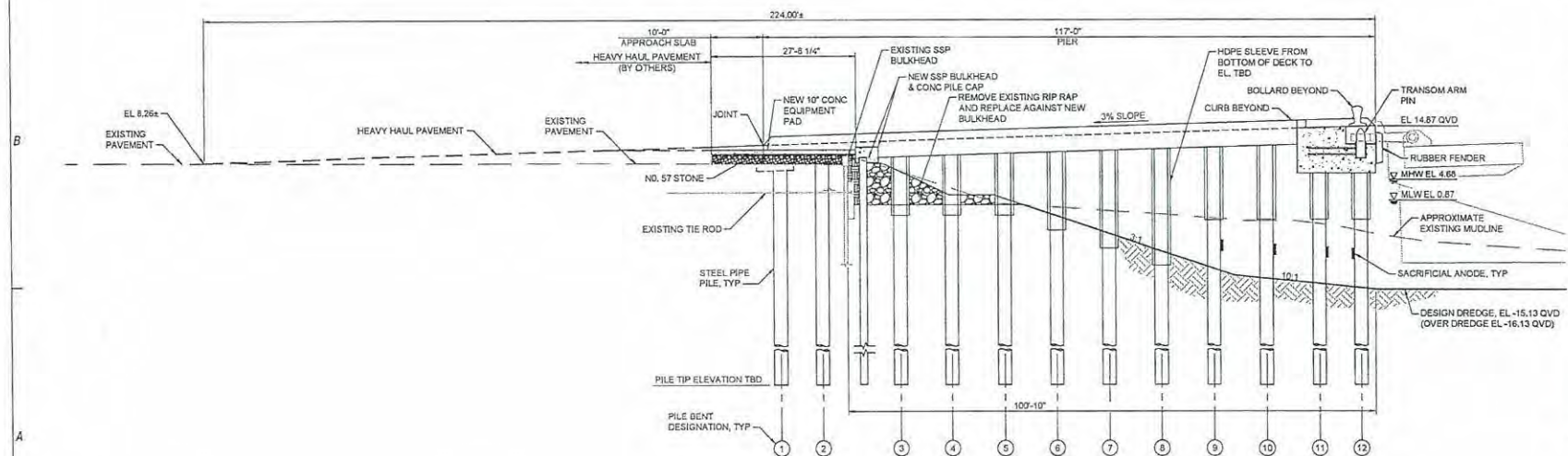
Quonset Business Park

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| SHEET NO. S-104 | DRAWING NO. 13 |
| 13 OF 18 SHEETS | |

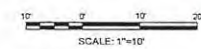




C-1 LONGITUDINAL SECTION - THROUGH CL OF PIER
SCALE: 1"=10'



A-1 LONGITUDINAL SECTION - THROUGH YAW-SWAY PIN
SCALE: 1"=10'



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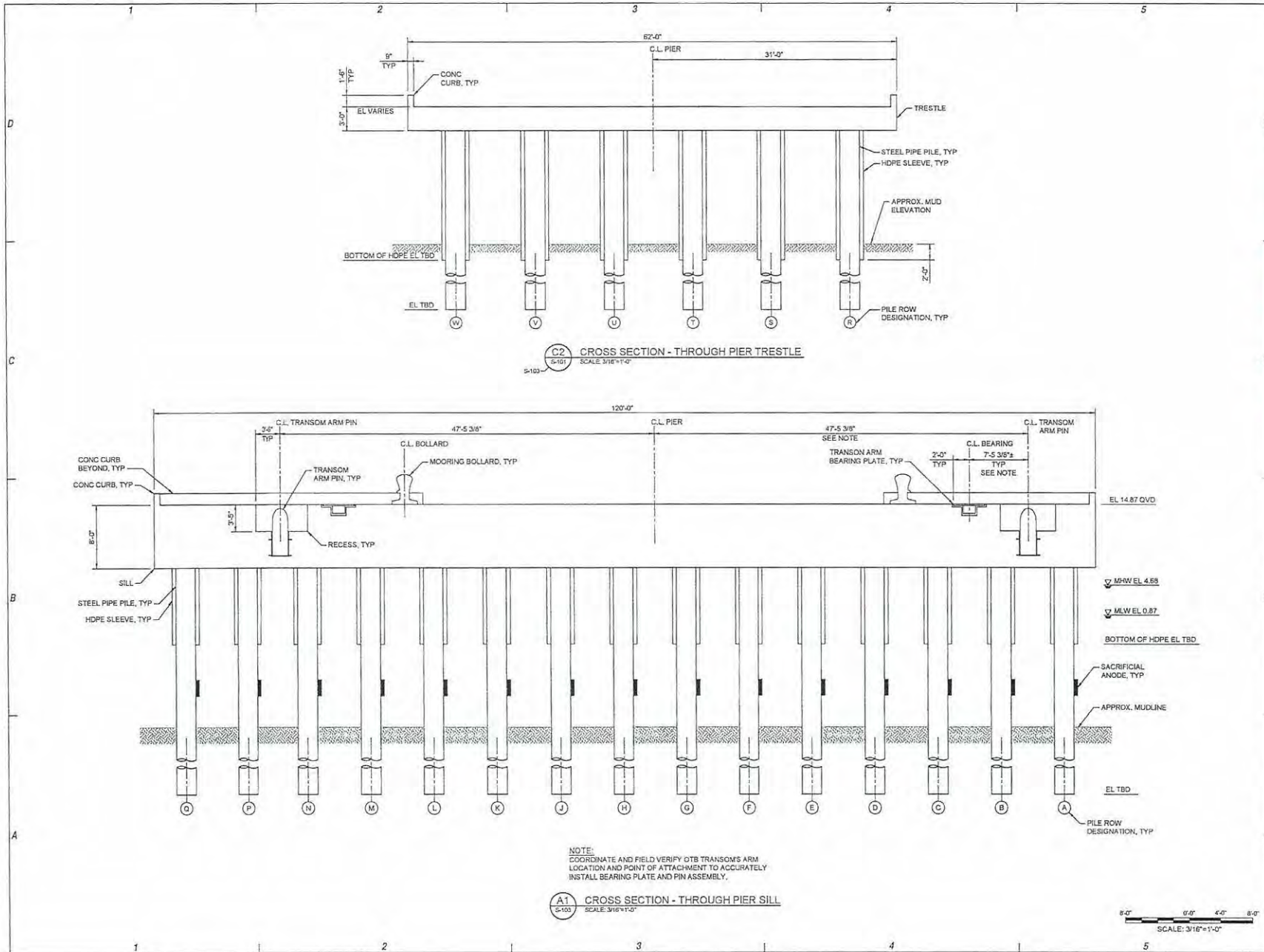
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| DESIGN BY: JAB | CHECKED BY: PRG |
| DRAWN BY: RDM | ENGINEER: JSC |
| SCALE: NOTED | PROJECT NO: 10763 |
| CONTRACT NO.: | |
| FILE NAME: 10763-01-S-301.DWG | |

TYPICAL
LONGITUDINAL PIER
SECTION

Quonset Business Park,

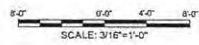
SHEET NO. **S-301** DRAWING NO. 14
14 OF 18 SHEETS



C2 CROSS SECTION - THROUGH PIER TRESTLE
SCALE: 3/16"=1'-0"

A1 CROSS SECTION - THROUGH PIER SILL
SCALE: 3/16"=1'-0"

NOTE:
COORDINATE AND FIELD VERIFY OTB TRANSOM'S ARM
LOCATION AND POINT OF ATTACHMENT TO ACCURATELY
INSTALL BEARING PLATE AND PIN ASSEMBLY.



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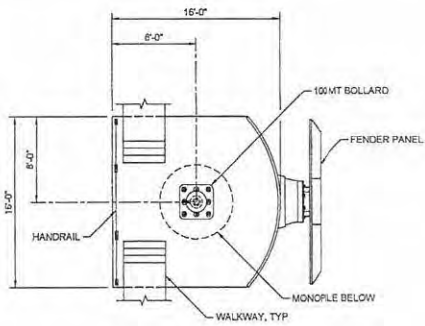
PERMIT SUBMITTAL
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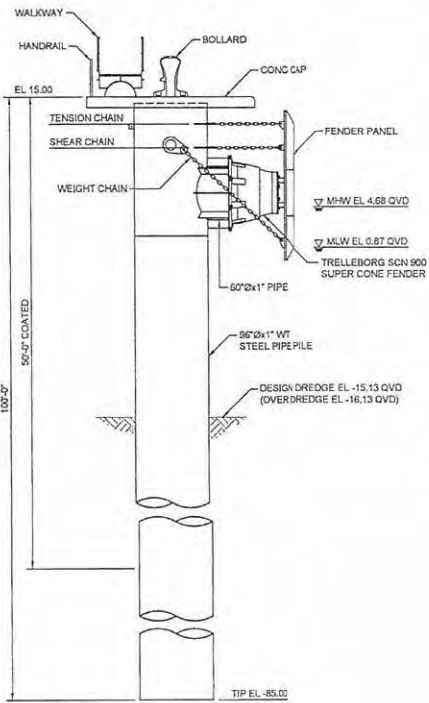
TYPICAL PIER CROSS SECTIONS

Quonset Business Park

SHEET NO. **S-302**
DRAWING NO. 15
15 OF 14 SHEETS



PLAN



FRONT ELEVATION

A1 MONOPILE MOORING DOLPHIN
NOT TO SCALE



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| DESIGN BY: JAB | CHECKED BY: PRG |
| DRAWN BY: RDM | ENGINEER: JSC |
| SCALE: NOTED | PROJECT NO: 10763 |
| CONTRACT NO. . . | |
| FILE NAME: 10763-05-508.DWG | |

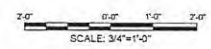
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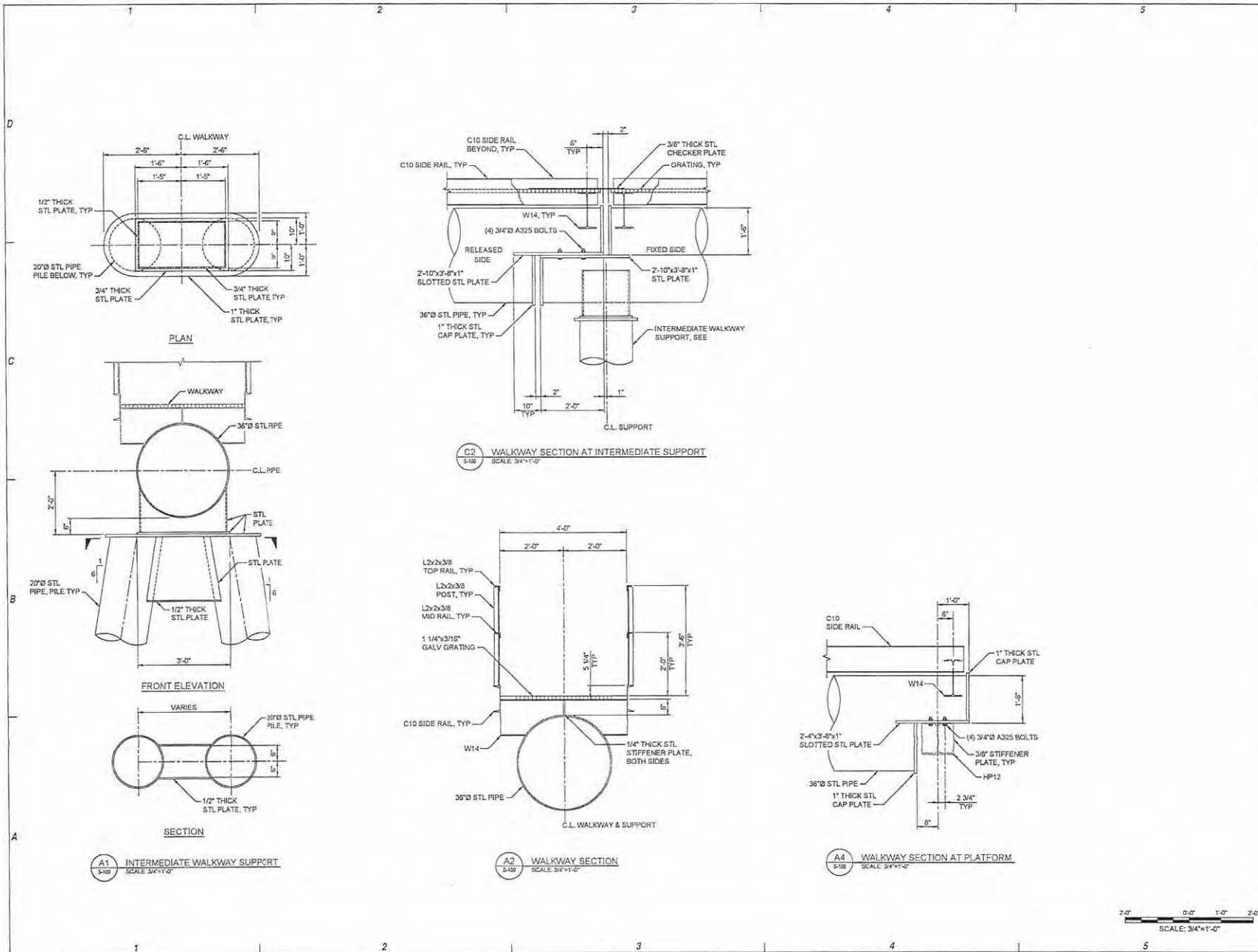
DRAWING TITLE

**MOORING DOLPHIN
DETAILS**

Quonset Business Park,

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| SHEET NO. S-508 16 OF 16 SHEETS | DRAWING NO. 16 |
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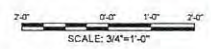
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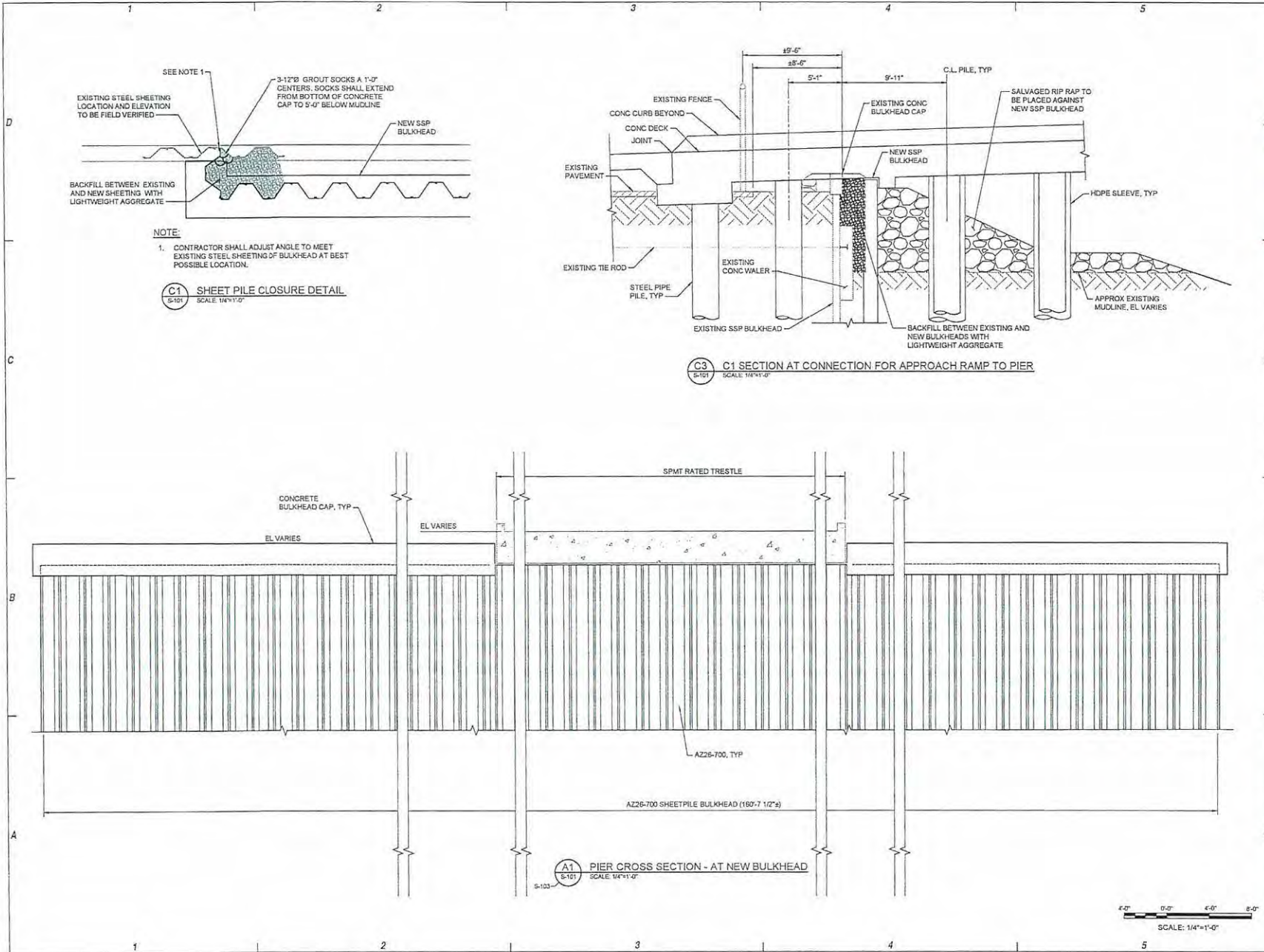
DRAWING TITLE

**MOORING WALKWAY
 DETAILS**

Quonset Business Park,

SHEET NO. **S-511** DRAWING NO. 17
 17 OF 18 SHEETS





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DESIGN BY: JAB CHECKED BY: PRG
 DRAWN BY: RDM ENGINEER: JSC
 SCALE: NOTED PROJECT NO. 10163
 CONTRACT NO. .
 FILE NAME: 10163-01-5-513.DWG

DRAWING TITLE
**TYPICAL BULKHEAD
 DETAILS**

Quonset Business Park,

SHEET NO. **S-513**
 18 OF 18 SHEETS
 DRAWING NO. **18**